



**CITY OF CORNING
PLANNING COMMISSION AGENDA**

**TUESDAY, JANUARY 18, 2011
CITY COUNCIL CHAMBERS
794 THIRD STREET
CORNING, CA 96021**

A. CALL TO ORDER: 6:30 p.m.

B. ROLL CALL:

**Commissioners: Robertson
Reilly
Hatley
Barron
Chairman: Lopez**

C. MINUTES:

1. Waive the Reading and Approve the Minutes of the November 16, 2010 Planning Commission Meeting with any necessary corrections.

D. BUSINESS FROM THE FLOOR: If there is anyone in the audience wishing to speak on items not already set on the Agenda, please come to the podium, and briefly identify the matter you wish to have placed on the Agenda. The Commission will then determine if such matter will be placed on the Agenda for this meeting, scheduled for a subsequent meeting, or recommend other appropriate action. If the matter is placed on tonight's Agenda, you will have the opportunity later in the meeting to return to the podium to discuss the issue. The law prohibits the Commission from taking formal action on the issue, however, unless it is placed on the Agenda for a later meeting so that interested members of the public will have a chance to appear and speak on the subject.

E. PUBLIC HEARINGS AND MEETINGS: Any person may speak on items scheduled for hearing at the time the Chairman declares the Hearing open. ALL LEGAL NOTICES PUBLISHED IN ACCORDANCE WITH LAW.

**2. Use Permit 2010-260 - Blue Beacon International, Inc.: Relocate the existing Blue Beacon Truck Wash from its current location along the southside of South Avenue to an undeveloped 3+ acre parcel on the westside of Hwy. 99 W, approximately 800 feet north of the South Avenue/Hwy. 99-W intersection.
APN: 87-040-70.**

F. REGULAR AGENDA: All items listed below are in the order which we believe are of most interest to the public at this meeting. However, if anyone in the audience wishes to have the order of the Agenda changed, please come to the podium, and explain the reason you are asking for the order of the Agenda to be changed.

G. ITEMS PLACED ON THE AGENDA FROM THE FLOOR:

H. ADJOURNMENT:

POSTED: THURSDAY, JANUARY 13, 2011



**CITY OF CORNING
PLANNING COMMISSION MINUTES
TUESDAY, NOVEMBER 16, 2010
CITY COUNCIL CHAMBERS
794 THIRD STREET
CORNING, CA 96021**

A. CALL TO ORDER: 6:30 p.m.

B. ROLL CALL:

**Commissioners: Robertson
Reilly
Hatley
Barron
Chairman: Lopez**

All Commissioners were present except Commissioner Hatley.

C. MINUTES:

1. Waive the Reading and Approve the Minutes of the September 21, 2010 Planning Commission Meeting with any necessary corrections.

Commissioner Reilly moved to approve the Minutes as written and Commissioner Barron seconded the motion. **Ayes: Lopez, Robertson, Reilly and Barron. Opposed: None. Absent: Hatley. Abstain: None. Motion approved by a 4-0 vote with Hatley absent.**

D. BUSINESS FROM THE FLOOR: None.

E. PUBLIC HEARINGS AND MEETINGS: Any person may speak on items scheduled for hearing at the time the Chairman declares the Hearing open. ALL LEGAL NOTICES PUBLISHED IN ACCORDANCE WITH LAW.

2. Revised Use Permit 2009-257, House of Brews; Approve or deny a revision to Use Permit 2009-257, House of Brews that would allow live acoustic/minimal amplification music on Friday nights from 6 to 9 PM. Address: 615 4th St.; APN: 71-116-06.

Chairman Lopez introduced this item by title stating the location and name of the petitioner. Planning Director Stoufer presented the Commission with a brief explanation of what is being requested. Mr. Stoufer stated that Mr. Holden approached Staff requesting a revision of his existing Use Permit that would allow live acoustic/minimal amplification music on Friday nights from 6 to 9 p.m. at his business, House of Brews. He further stated that neighbors within 500 feet of the business location were notified of the requested revision and to date the City has received no responses. He also informed the Commission that Mr. Holden has had a few people in playing music and have received no complaints to date.

Mr. Stoufer said Staff is recommending adding one additional Condition to the original 2 listed on the existing Use Permit. **Condition No. #3 is: Live music will only be allowed to be played on Friday nights from the hours of 6:00 p.m. to 9:00 p.m. Amplification of the music may not exceed 70dB at the residence located at the southwest corner of the 4th Street/Butte Street intersection. If there is a question as to the noise level at this residence it will be the responsibility of the applicant to provide information from an accoustical engineer or other qualified professional to assure that the noise level does not exceed 70 dB.** Mr. Stoufer also stated that the petitioner is present to answer any questions. Chairman Lopez then opened the public hearing.

Commissioner Robertson stated she had stopped by and saw the room; it is very small. Commissioner Barron asked if he expected this to occur every Friday night and was informed no, possibly every other Friday night.

Commissioner Robertson moved to close the Public Hearing and Commissioner Barron seconded the motion. **Ayes: Lopez, Robertson, Reilly and Barron. Opposed: None. Absent: Hatley. Abstain: None. Motion approved by a 4-0 vote with Hatley absent.**

Commissioner Robertson moved to adopt the four Findings and Subfindings as presented in the Staff Report and approve a revision to Use Permit 2009-257 allowing live acoustic minimal amplification music on Friday nights from 6 to 9 p.m. subject to the two original Conditions and with the addition of Condition No. 3 (as highlighted above). Commissioner Reilly seconded the motion. **Ayes: Lopez, Robertson, Reilly and Barron. Opposed: None. Absent: Hatley. Abstain: None. Motion approved by a 4-0 vote with Hatley absent.**

F. REGULAR AGENDA:

3. Study Matter 2010-1, Discuss Amending the Outdoor Sign Regulations

Discuss adopting a resolution to amend the "Outdoor Advertising Sign Regulations" allowing off-site directional or wayfinding signs for locating businesses within the City.

Chairman Lopez introduced the matter to be discussed. Planning Director Stoufer stated that this is a study matter only; no action will be taken tonight. This matter will come before the Planning Commission at a later date for action. Mr. Stoufer briefed the Commission on "Branding" as presented by Mr. Brooks to entice travelers to City businesses. He also stated that currently City Ordinances do not allow off-site signage. He stated it would be beneficial to local businesses to provide signage at the two off-ramps from I-5 (South Avenue and Corning Road) that directs potential customers to the businesses located in the City. Mr. Stoufer contacted and informed Chamber of Commerce Manager Valanne Cardenas of the tonight's discussion topic with the hope she would inform local businesses owners and seek their attendance at tonight's meeting, as we want their input. He stated he was happy with the number of representative present.

Chairman Lopez stated that the City should continue working with the businesses on methods to capture more business from those utilizing the freeway.

Commissioner Robertson asked where the sign funding would come from and what method would be used to determine which businesses would be advertised on the signs? Mr. Stoufer stated that the businesses would probably be required to pay their share of the signage; the amount would have to be determined. He emphasized that we are still in the infant stage of planning.

Dewey Lucero of Lucero Olive Oil suggested, (as Roger Brooks suggests) to start the signage with the well known existing businesses in the first stage. These businesses will bring people in, and those people will in turn bring people to other local businesses that are not as well known in other areas of the City. Mr. Stoufer clarified this as "Anchor Businesses" (as Mr. Brooks calls them).

Business owner Bruce Christensen (Miracle Apholstry) stated that his shop is two blocks south of Solano on 4th Street. He currently has no signage and would love to get some kind of signage on Solano Street directing people to his business.

Melodie Poisson of DM-Tech stated that she really likes this idea a lot. She stated that she is visualizing the directional signs throughout town that would create a style, a design having that quaint feeling with a theme so that they all match. She also stated that she agrees with Commissioner Barron in that the signs should be perpendicular to the street.

Commissioner Robertson suggested a "Theme Sign Design" contest with some type of prize. Mr. Stoufer suggested that possibly the Chamber could assist with this.

Commissioner Reilly stated that he believes the business owners present are in favor of pursuing this type of signage, as is he. He feels we should move forward with this quickly.

****Commissioner Barron stated that he would like to see at the next meeting what the current Ordinances/Regulations are.**

Valanne Cardenas suggested having three or four businesses on each signpost at the intersections on Solano. Dewey Lucero suggested having the main signage at the off-ramps with the other signs placed along Solano Street, in other words two types of signs.

Mr. Stoufer stated that he would look into what other Cities are doing and informed the Commission that this item probably won't come back before the Commission in December, but rather in January.

Chairman Lopez asked what the procedure would be for advertising at the I-5 Rest Area? He stated that he thinks it would be especially beneficial to our local businesses and that we should contact Caltrans to find out if that would be possible.

Don Holden stated many people travel looking for historical sites and suggested signage that advertises the Corning Museum, especially since you have to go through most of the town to get there. Commissioner Robertson stated that it would be nice to have the big Olive that is located on South Avenue in Corning. Mr. Stoufer suggested possibly getting Mr. Brooks in to work with us on this.

Louis Davies stated that many people get lost trying to find Hwy. 99 W since there really isn't a Hwy. 99-W street sign other than at Solano Street and Highway 99-W/Edith Avenue.

Paulyne White suggested that it might be beneficial to get all the local businesses here to further discuss this and offered to form a group to hand out flyers to the local businesses. Commissioner Robertson suggested taking someone bilingual with them when contacting the business owners.

Chairman Lopez stated that he would like to get as many people involved as possible, the more the better.

Julie Johnson stated that it is a really good idea to draw people into the downtown, however there are so many of the downtown business frontages in the older section of town that look really bad. She further stated that if we plan on drawing people into the Community we really need to do something with about this.

Chairman Lopez stated that some of these businesses at this time might not be able to financially afford to change their business façade. However, with time and through the process, programs might be found to help finance façade improvement through grants, etc. He emphasized that this won't happen overnight, but through small steps over time.

Mr. Stoufer stated that if the Commission agrees, he would work with Valanne Cardenas and keep the Commission and Business owners informed. By consensus the Commission agreed.

G. ITEMS PLACED ON THE AGENDA FROM THE FLOOR: None.

H. ADJOURNMENT: 7:25 p.m.

Lisa M. Linnet, City Clerk

**ITEM NO: E-2
USE PERMIT APPLICATION 2010-260;
BLUE BEACON INTERNATIONAL, INC.
CONSTRUCT A NEW TRUCK WASH
FACILITY WITH TWO WASH BAYS,
EQUIPMENT ROOM AND OFFICE SPACE TO
REPLACE THE EXISTING TRUCK WASH
LOCATED ALONG SOUTH AVE. THE
PROPOSED NEW LOCATION IS ON THE
WEST SIDE OF HWY 99W, APPROXIMATELY
800 FEET NORTH OF THE SOUTH AVE. /
HWY 99W INTERSECTION.
APN: 87-040-70 ADDRESS: 3000 Hwy 99W**

JANUARY 18, 2011

TO: PLANNING COMMISSIONERS OF THE CITY OF CORNING

FROM: JOHN STOUFER; PLANNING DIRECTOR

PROJECT DESCRIPTION:

Construct a new truck wash facility with two 3,110 sq. ft. wash bays, a 1,357 sq. ft. equipment room, and 1,013 sq. ft. office space area. Additional site development will include asphalt drives for entering and exiting the site and wash bays along with an employee and customer car parking lot, a 1,300 sq. ft. drying basin, landscape berms along the north and south property lines, and two detention ponds. This facility will replace an existing truck wash operated by Blue Beacon International, Inc. located along the south side of South Ave. approximately 1000 feet east of the South Ave. / Hwy 99W intersection. The proposed location for the new truck wash is on the west side of Hwy 99W, approximately 800 feet north of the South Ave. / Hwy 99W intersection.
APN: 87-040-70

DISCUSSION:

Blue Beacon International, Inc. currently operates a truck wash facility along the south side of South Avenue, just east of the Petro Truck Stop. They currently lease the property and building from the owners of the land where the Petro Truck Stop is located. Instead of renewing their lease agreement they have purchased an approximately 3.12 acre parcel, referenced as APN: 87-040-70, and have applied to construct a new truck wash facility as described above on the parcel.

The lease agreement allows the existing truck wash building to remain on the property if so desired by the property owners but does prohibit the existing building from being used as a truck wash once Blue Beacon vacates the site. Staff has not been informed as to any future plans for the use of the building. Pursuant to the requirements set forth in the Zoning Code a changed use of the building will require the issuance of a Conditional Use Permit approved by the Planning Commission.

GENERAL PLAN LAND USE DESIGNATION

Hwy99-W - Highway 99W Corridor Specific Plan. The Highway 99W Corridor Specific Plan is intended to provide a more detailed examination of the planning issues in the corridor than could be achieved in the City's General Plan. The purpose of the Specific Plan is to provide a comprehensive set of plans, policies, guidelines, and implementation measures for guiding and ensuring the orderly development of the Highway 99W corridor.

ZONING

CH –CBDZ, Highway Service Commercial – Corning Business Development Zone. The CH Zone is intended to be applied along main road and highway frontages at proper intervals and locations to provide necessary services for the traveling public in developments designed for safety, convenience and fitting appearance. The CBDZ Zone has been established to achieve the following purposes:

- A. To protect the public health, safety and welfare by enhancing quality of life and improving the appearance of the city;
- B. To provide protection against haphazard and traditional strip commercial development by implementing visual design guidelines established in the Highway 99W corridor specific plan;
- C. To allow site development flexibility in return for well-conceived and efficient site planning and quality design;
- D. To establish overlay districts that carry out specific purposes prescribed by the Highway 99W corridor specific plan addressing specific subjects, such as freeway-oriented commercial development and mixed-use commercial development.

CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA)

Mitigated Negative Declaration, A mitigated negative declaration means a negative declaration prepared for a project when the initial study has identified potentially significant effects on the environment, but (1) revisions in the project plans or proposals made by, or agreed to by, the applicant before the proposed negative declaration and initial study are released for public review would avoid the effects or mitigate the effects to a point where clearly no significant effect on the environment would occur, and (2) there is no substantial evidence in light of the whole record before the public agency that the project, as revised, may have a significant effect on the environment.

Staff completed a CEQA Initial Study that identified certain potentially significant effects that could be attributable to the development of this project. Staff determined that those effects could be substantially lessened by the imposition of sixty-eight (68) mitigation measures and conditions.

MITIGATION MEASURES VS. CONDITIONS. The CEQA Mitigated Negative Declaration/Initial Study recommends both “Mitigation Measures” and “Conditions”. Mitigation Measures” are those measures recommended to mitigate or avoid specific environmental effects. “Initial Study Conditions” are measures to ensure compliance with applicable development standards. The recommended “Conditions of Approval” in this staff report include both types of measures (Mitigation Measures and Conditions). Please refer to the Mitigated Negative Declaration/Initial Study (Exhibit “P”) for identification of the potential significant environmental effects and the discussion regarding appropriate mitigation and compliance with City standards.

CONSISTENCY WITH GENERAL PLAN

LAND USE ELEMENT:

The site was designated Commercial as shown on the Current Land Use Map for the Highway 99W Specific Plan area. Development of a commercial use such as a truck wash at this location is consistent with the following Land Use Policies of the Corning General Plan and the Hwy. 99W Corridor Specific Plan.

Policy #6 – Encourage the location and development of businesses which generate high property and sales taxes, local employment and are environmentally compatible.

Policy #7 – Commercial development should be clustered on arterial streets and at major intersections in the downtown or near Interstate 5 interchanges

CIRCULATION:

Highway 99W. is classified as an arterial road in the Circulation Element, therefore the establishment of truck wash facility along an arterial road is consistent with the General Plan and the Highway 99W. Corridor Specific Plan.

SAFETY

The Safety and Public Facilities Element is intended to identify risks from hazards or safety problems within the specific plan area, and to provide an assessment of existing protection services and the impact future development may have on these services. The most significant hazard, identified at the time the specific plan was prepared, was the Dudley Truckstop located on approximately 13.2 acres east of the site. This site was contaminated with petroleum from underground leaking tanks. Action was taken to remove and remediate the contaminated soils and the site is now the location of the Flying J Truckstop.

A portion of the site falls within the AO Zone as designated on the Federal Emergency Management Agency (FEMA) Maps. The AO Zone means that these are areas of shallow flooding where depths are between 1 and 3 feet. The last major flooding events were in 1995 and 1997. In 1995 some areas within the specific plan area were subject

to 6" to 1' of water. In 1997 no flooding occurred within the specific plan area. Increased maintenance of drains and streams has lessened the impact of flood waters in this area.

NOISE

The General Plan Noise Element identified that normally acceptable range for office and commercial land uses is an Ldn value below 70 db (Ldn values of 67.5 to 77.5 are conditionally acceptable). Ldn is described as a statistical weighting of daytime and nighttime noises and is used as the basis of noise impact evaluation for land use planning. The decibel (db) scale is a logarithmic scale used to measure sound intensities. Mitigation Measures have been implemented to reduce the impacts of developing the site as a truck wash so that the decibel level will not significantly increase above existing levels at the site.

The project is consistent with the following noise element goals, policies, and implementation measures of the Hwy. 99-W Specific Plan.

Noise Goals

- Ensure that new development conforms to City noise levels.
- Locate new noise sensitive land uses away from noise sources unless mitigation measures are included in development plans.

Noise Policies

- Establish buffer areas between sensitive land uses and noise sources.

Noise Implementation Measures

- Implement staff and planning commission review of potential noise issues in new project location and design features.
- Incorporate the noise mitigations identified in initial studies for new projects as conditions of approval.

RECOMMENDATION:

Staff recommends that the Planning Commission adopt the following, or similar, Factual Subfindings and Legal Findings for Use Permit 2010-260:

Factual Subfinding #1

An Initial Study analyzing the environmental impacts associated with the project has been prepared, a Mitigated Negative Declaration filed and circulated through the CEQA process.

Legal Finding #1

The Corning Planning Commission finds that the Initial Study analyzed the environmental impacts associated with the project and that identified impacts have been mitigated to a Less than Significant Level. The Mitigated Negative Declaration filed on Use Permit 2010-260, meets the requirements of CEQA and its Guidelines.

Factual Subfinding #2

The site proposed for the development of a truck wash facility is located on a parcel that is zoned CH - CBDZ, Highway Service Commercial - Corning Business Development Zone.

Legal Finding #2

The establishment of a truck wash facility is a similar use, as vehicle washing facilities are often times established as a part of a gasoline service station therefore, a truck wash facility is an allowed use in a CH -CBDZ Zoning District pursuant to the granting of a conditional use permit by the Planning Commission.

Factual Subfinding #3

The parcel proposed for a truck wash is approximately 3.12 acres in size, has less than a 2% slope in topography, and has an appropriate shape for the proposed development.

Legal Finding #3

The parcel proposed for development of a truck wash facility is adequate in size, shape and topography.

Factual Subfinding #4

The parcel has approximately 225 feet of frontage and direct access to Highway 99W.

Legal Finding #4

The site has sufficient access to Hwy. 99W. and frontage improvements will be constructed to provide adequate width, pavement and capacity for the development of a truck wash facility.

Factual Subfinding #5

Blue Beacon International, Inc. proposes to establish a truck wash facility in an area, the South Avenue Interchange that is established with existing businesses serving truck traffic along Interstate 5 within the Hwy 99W Corridor Specific Plan.

Legal Finding #5

The establishment of a truck wash facility will not have an adverse effect upon the use, enjoyment or valuation of adjacent or neighboring properties or upon the public welfare.

ACTION

1. **MOVE TO ADOPT THE 5 FACTUAL SUBFINDINGS AND LEGAL FINDINGS AS PRESENTED IN THE STAFF REPORT FOR USE PERMIT 2010-260**

(PLEASE NOTE : PRIOR TO ADOPTING THE RECOMMENDED SUBFINDINGS & FINDINGS THE COMMISSION HAS THE ABILITY TO MODIFY OR REMOVE ANY OF THE SUBFINDINGS AND FINDINGS IF DEEMED APPROPRIATE BY A MAJORITY OF THE COMMISSION)

VOTE OF THE COMMISSION

2. **MOVE TO ADOPT THE MITIGATED NEGATIVE DECLARATION FILED FOR USE PERMIT 2010-260 AND APPROVE USE PERMIT 2010-260 SUBJECT TO THE 36 CONDITIONS OF APPROVAL AS RECOMMENDED BY STAFF. (PLEASE NOTE: THE COMMISSION HAS THE ABILITY TO MODIFY, DELETE OR ADD CONDITIONS PRIOR TO APPROVAL OF THE PROJECT.)**

VOTE OF THE COMMISSION

OR:

Failing to make findings in support of the project recommend findings in denial of the project for consideration by the Commission.

Adopt findings in denial of the project and deny Use Permit 2010-260.

**STAFF RECOMMENDS THE FOLLOWING
CONDITIONS OF APPROVAL
FOR USE PERMIT 2010-260:**

CONDITION #1 – AGENCY COMPLIANCE:

The development and continued operation of a truck wash facility, permitted pursuant to Use Permit 2010-260, must comply with all applicable local, state, and federal laws and regulations.

CONDITION #2 – HANDICAPPED PARKING:

Prior to issuance of a certificate of occupancy for the truck wash facility the applicant must designate and appropriately mark handicapped parking spaces as approved by the Building Official.

CONDITION #3 SIGN REGULATIONS

The truck wash facility must comply with the City of Corning sign regulations established by Resolution 10-25-05-01.

CONDITION #4 – LANDSCAPING PLANS (Mitigation Measure I. C. 1):

Prior to commencing construction activities associated with the development of the site as a truck wash, the applicant or his engineer shall submit landscaping and signage plans that comply with the landscaping design guidelines and sign design guidelines of the Highway 99W Corridor Specific Plan. The landscaping plans must include irrigation plans that comply with the "Water Efficient Landscape Regulations" pursuant to Section 15.08.055 of the Corning Municipal Code. Both plans must be approved by city staff prior to installation and the sign installation and landscaping must be completed prior to the city issuing a certificate of occupancy for the truck wash buildings.

CONDITION #5 – UNDERGROUND UTILITIES:

Underground existing telephone and CATV along the Highway 99-W frontage and remove existing utility pole as shown on the Improvement Plans, Utility Plan Sheet UP5, submitted with the Use Permit application.

CONDITION #6- REMOVE CONSTRUCTION DEBRIS:

Prior to the issuance of a Certificate of Occupancy for the truck wash buildings all construction debris must be removed from the site.

CONDITION #7 - FUGITIVE DUST PERMIT (Mitigation Measure III. B. 1):

Prior to commencement of any type of construction activities the applicant must submit a construction emission dust/control plan and obtain a Fugitive Dust Control Permit from the Tehama County Air Pollution District and comply with the conditions of approval.

CONDITION #8 - OPEN BURNING (Mitigation Measure III. B. 2):

No opening burning shall occur on this parcel unless a special land clearing permit is obtained from the Tehama County Air Pollution Control District.

CONDITION #9 - SPRINKLE EXPOSED SOILS (Mitigation Measure III. C. 1):

During construction, unprotected or bare soils, including inactive storage piles, shall be watered a minimum of 2 times per day to minimize wind erosion. Frequency should be based upon the type of operation, soil, and wind exposure.

CONDITION #10 - COVER EXPOSED SOILS (Mitigation Measure III. C. 2):

Areas denuded by construction activities and not scheduled for development for an indefinite period shall be seeded or covered by impervious materials to minimize water and wind erosion prior to the beginning of the rainy season (October 15th).

CONDITION #11 - GRADING PLANS:

Complete grading plans shall be submitted for approval by the City Engineer.

CONDITION #12 - STREET CLEANING :

Paved City roadways leading to or from the project area shall be swept or washed at the end of each day as necessary to remove excessive accumulations of silt and/or mud, which may have accumulated as the result of construction activities.

CONDITION #13 - CULTURAL RESOURCES (Mitigation Measure V. A. 1):

If subsurface deposits believed to be cultural in origin are discovered during construction, then all work must halt within a 100-foot radius of the discovery, and the City of Corning notified. A qualified professional archaeologist, meeting the Secretary of the Interior's Professional Qualification Standards for prehistoric and historic archaeologist, shall be retained to evaluate the significance of the find. Work cannot continue at the discovery location until the archaeologist conducts sufficient research and data collection to make a determination that the resource is either 1) not cultural in origin; or 2) not potentially significant. If a potentially-eligible resource is encountered, then the archaeologist, lead agency, and project proponent shall arrange for either 1) total data recovery as a mitigation, or, preferably, 2) total avoidance of the resource, if possible. The determination shall be formally documented in writing and submitted to the lead agency as verification that the provisions in CEQA for managing unanticipated discoveries have been met.

CONDITION #14 - HUMAN REMAINS (Mitigation Measure V. D. 1):

If human remains, or remains that are potentially human, are discovered during project construction or implementation, all work must stop within a 100-foot radius of the find. The construction supervisor must notify the Corning Police Department immediately, and take appropriate action to ensure that the discovery is protected from further disturbance or vandalism.

CONDITION #15 - STORMWATER PERMIT (Mitigation Measure VI. B. 1):

Applicant shall apply for and obtain a "Construction Activities Storm Water General Permit" from the State Water Resources Control Board, Central Valley Regional Water Quality Control Board.

CONDITION #16 - STORMWATER POLLUTION PREVENTION PLAN (Mitigation Measure VI. B. 2):

Prior to any site disturbance or earthmoving activities on or adjacent to the site, a construction period and post-construction period Storm Water Pollution Prevention Plan (SWPPP) shall be prepared and presented to the Central Valley Regional Water Quality Control Board and approved by the City of Corning. The objective of the plan shall be no net loss of soil (above an undisturbed natural, stable background state) from the site due to erosion. All requirements of the post construction period SWPPP shall be completed as part of the required improvement plans and shall be maintained in the same manner.

CONDITION #17 - SOILS INVESTIGATION:

Prior to the issuance of a building permit for the truck wash facility the applicant shall submit a soils investigation by a registered engineering geologist or civil engineer to determine if expansive soils requiring special foundation design is necessary. The developer shall provide: 1) certification assuring adequate compaction of filled lots in accordance with the Uniform Building Code; and 2) for those lots with expansive soils, certification that the engineered foundation plans comply with building code requirements.

CONDITION #18 - BUILDING EFFICIENCY (Mitigation Measure VII. A. 1):

Increase building energy efficiency rating by 10% above what is required by Title 24 requirements. This can be accomplished in a number of ways (increasing attic, wall or floor insulation, etc.)

CONDITION #19 - THERMAL EFFICIENCY (Mitigation Measure VII. A. 2):

Improvement of thermal efficiency of the office area by reducing the thermal load with automated and timed temperature controls.

CONDITION #20- HAZARDOUS MATERIALS STORAGE

(Mitigation Measure VIII. B. 1):

The operators of the truck wash must submit a signed Hazardous Materials general information questionnaire to the Tehama County Department of Environmental Health and if business handles hazardous materials above the threshold quantities a business emergency response plan and inventory shall be submitted to the Department of Environmental Health at least 30 days prior to commencing operation.

CONDITION #21 - FINISHED GRADE:

Finished grade must be graded to direct runoff to stormwater drain facilities within the public right-of way or established drainage facilities (detention basins) constructed on the parcel. No lot-to-lot runoff is permitted.

CONDITION #22 - DETENTION BASINS:

The finished grading plan and detention basin size must be approved by the City Engineer prior to the commencement of any grading of the site.

CONDITION #23 - CONSTRUCTION HOURS (Mitigation Measure XI. D. 1):

Excavation and construction work shall occur only between the hours of 7:00 AM to 7:00 PM, Monday through Friday, and between the hours of 8:00 AM to 6:00 PM on weekends and federally observed holidays.

CONDITION #24 - CONSTRUCTION EQUIPMENT (Mitigation Measure XI. D. 2):

The primary contractor shall be responsible for ensuring that all construction equipment is properly tuned and maintained. When feasible, existing power sources, such as power poles, or clean fuel generators should be used, rather than temporary power generators. Minimize idling time to 10 minutes.

CONDITION #25 - NOISE REDUCTION (Mitigation Measure XI. C. 1):

A minimum two feet high berm along the north and south property lines, with trees planted on top as shown on the site plans submitted with the Use Permit application, must be installed prior to the City issuing a Certificate of Occupancy for the truck wash.

CONDITION #26 - DEVELOPMENT IMPACT FEES:

Development of the truck wash at this location is subject to imposed in order to lessen new development's impacts on City facilities and services. These fees shall be assessed and payable prior to issuance of a Certificate of Occupancy. Costs incurred for improvements to "backbone" infrastructure facilities shall be credited against development impact fees.

The City Council may consider and approve modifications to development impact fees related to sewer and water system impacts. Any such modifications shall be based on an engineer's report(s) and confirmed through actual monitoring of sewer and/or water usage.

CONDITION #27 - HIGHWAY 99W IMPROVEMENTS:

Construct the adjacent (west) half width of Highway 99W in accordance with the adopted standard (including acceleration lane, travel lane, median turn lane) and complete asphalt overlay of a 14 foot travel lane on the opposite (east) half width along property frontage with appropriate tapering north and south of the project site.

CONDITION #28 - CURB, GUTTER, & SIDEWALK IMPROVEMENTS:

Install curb, gutter and sidewalk, with approved handicap ramps in accordance with adopted Highway 99W Specific Plan standard.

CONDITION #29 - THERMOPLASTIC STRIPING:

Apply thermoplastic striping for median turn lane and acceleration lane as directed by the City Engineer.

CONDITION #30 - PUBLIC IMPROVEMENT STANDARDS:

All public improvements shall be constructed in accordance with the Subdivision Ordinance of the City of Corning and required Public Works Standards.

CONDITION #31 - WATER & SEWER CONNECTIONS:

Water and sewer connections shall be completed in accordance with Public Works Specifications. Install manhole at sewer lateral connection in Highway 99W.

CONDITION #32 - WATER METERS:

All water meters to be Sensus compound meters to register in gallons.

CONDITION #33 - WATER CONNECTIONS:

Water service connected to water main in Highway 99W, as per Public Works Specifications as shown on the Improvement Plans submitted with the application and dated 10/04/10

CONDITION #34 - SEWER CONNECTIONS: Sewer service connection to join gravity sewer truck line within Highway 99W right-of-way in accordance with Public Works Specifications as shown on Improvement Plans submitted with application and dated 10/04/10.

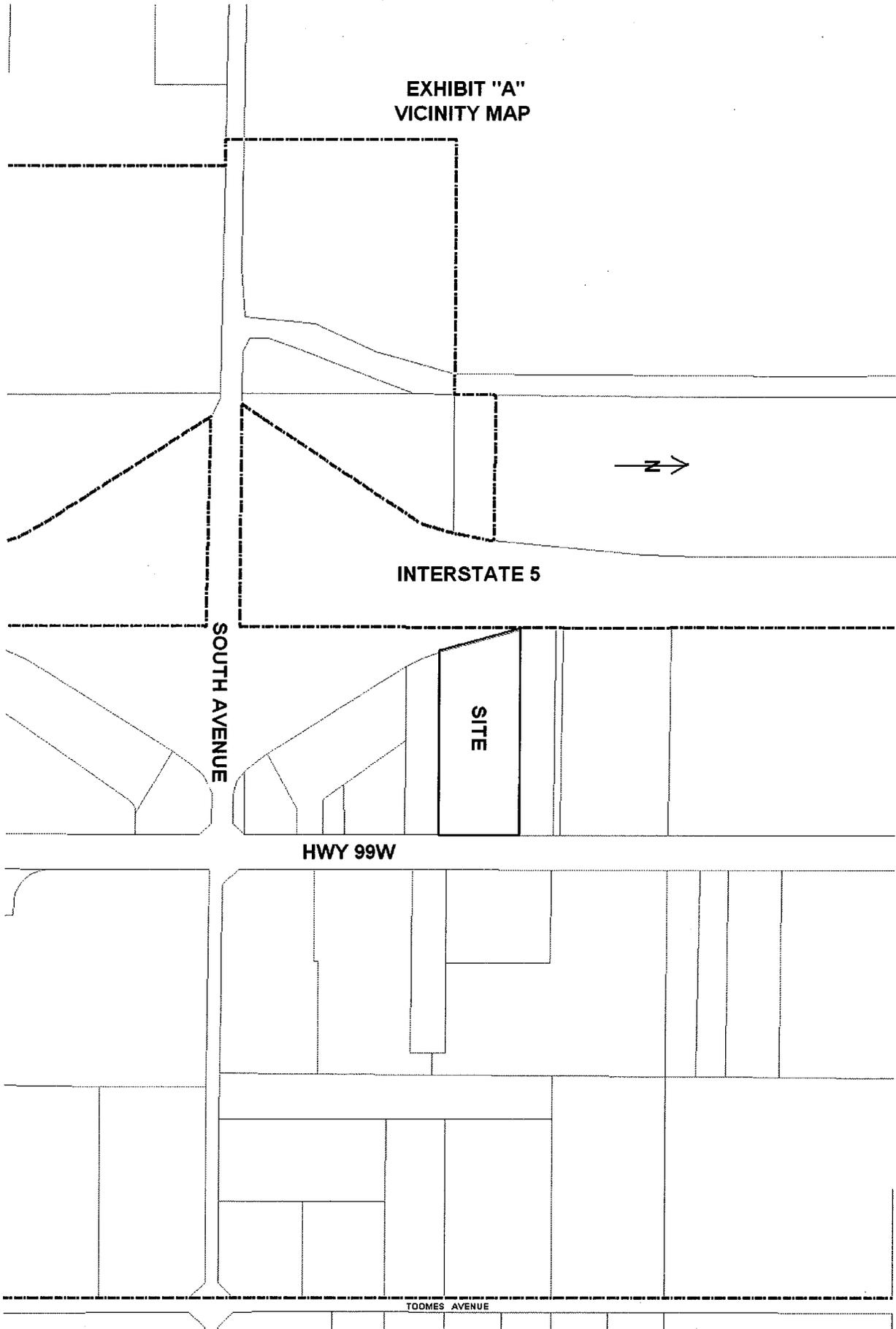
CONDITION #35 - ELECTRICAL SERVICE: Electrical service to the site shall be undergrounded from the existing pole on east side of Highway 99W as shown on the Improvement Plans, Utility Plan sheet UP5, submitted with the application and dated 10/04/10.

CONDITION #36 - STREET LIGHTING: Two street lights, positioned near the northeast and southeast property corners, respectively, shall be installed in accordance with Public Works Standards SL-1 and the City's Land Division Standards. Final location shall be shown on the plans for public improvements, and approved by the City Engineer.

ATTACHMENTS

Exhibit "A"	Vicinity Map
Exhibit "B"	Aerial Photo of Site
Exhibit "C"	Zoning Map
Exhibit "D"	General Plan Map
Exhibit "E"	Application with Preliminary Plan Drawings
Exhibit "F"	Mitigated Negative Declaration & Initial Study

**EXHIBIT "A"
VICINITY MAP**



INTERSTATE 5

SOUTH AVENUE

SITE

HWY 99W

TODMES AVENUE

EXHIBIT "B"
AERIAL PHOTO



EXHIBIT "C"
ZONING MAP

INTERSTATE 5



SITE

SP MD GR 60DZ

GR 60DZ

GR 60DZ

HWY 99W

SOUTH AVE

MD 60DZ

GR

R-1A

EXHIBIT "D"
GENERAL PLAN MAP

INTERSTATE 5

SITE

HWY 99 W

STATE ROUTE 99 W

PM

SOUTH AVENUE

6

1

A

HWY 99 W



CITY OF CORNING
PLANNING APPLICATION
 TYPE OR PRINT CLEARLY

Submit Completed Applications to:
 City of Corning
 Planning Dept.
 794 Third Street
 Corning, CA 96021

PROJECT INFORMATION	PROJECT ADDRESS 3000 Highway 99 W		ASSESSOR'S PARCEL NUMBER 87-040-70	G.P. LAND USE DESIGNATION Hwy 99W-Specific Plan
	ZONING DISTRICT CH-CBDZ	FLOOD HAZARD ZONE A0/X	SITE ACREAGE 3.12	AIRPORT SAFETY ZONE? N/A
	PROJECT DESCRIPTION: (attach additional sheets if necessary) Construct new Truck Wash / see attached sheets			
	APPLICATION TYPE (Check All Applicable)			
<input type="checkbox"/> Annexation/Detachment <input type="checkbox"/> General Plan Amendment <input type="checkbox"/> Lot Line Adjustment <input type="checkbox"/> Merge Lots <input type="checkbox"/> Planned Dev. Use Permit <input type="checkbox"/> Parcel Map <input type="checkbox"/> Preliminary Plan Review <input type="checkbox"/> Rezone <input type="checkbox"/> Street Abandonment <input type="checkbox"/> Subdivision <input type="checkbox"/> Time Extension <input checked="" type="checkbox"/> Use Permit				
APPLICANT INFORMATION	APPLICANT Blue Beacon International, Inc.		ADDRESS 500 Graves Blvd. Salins, KS 67401	DAY PHONE 785 825-2221
	REPRESENTATIVE (IF ANY) Butler Engineering Group		ADDRESS 8837 Airport Rd. Ste A Redding, CA 96002	DAY PHONE 530-222-5211
	PROPERTY OWNER Blue Beacon International, Inc.		ADDRESS same as above	DAY PHONE
	CORRESPONDENCE TO BE SENT TO <input checked="" type="checkbox"/> APPLICANT <input checked="" type="checkbox"/> REPRESENTATIVE <input type="checkbox"/> PROP. OWNER			
	APPLICANT/REPRESENTATIVE: I have reviewed this application and the attached material. The information provided is correct. Signed: _____		PROPERTY OWNER: I have read this application and consent to its filing. Signed: <u>Dana Moore</u> For Blue Beacon	
By signing this application, the applicant/property owner agrees to defend, indemnify, and hold the City of Corning harmless from any claim, action, or proceeding brought to attack, set aside, void or annul the City's approval of this application, and any Environmental Review associated with the proposed project.				

SUBMITTAL INFO	FOR OFFICE USE ONLY			
	APPLICATION NO. 2010-260	RECEIVED BY: JS	DATE RECEIVED 10/4/2010	DATE APPL. DEEMED COMPLETE
	FEE RECEIVED/RECEIPT NO. 51226 CK# 01157	CEQA DETERMINATION Exempt ND (MND) EIR		DATE FILED



CITY OF CORNING

ENVIRONMENTAL INFORMATION FORM (To be completed by Applicant)

DATE FILED _____

General Information

1. Project Title:

Blue Beacon of Corning

2. List and describe any other related permits and other public approvals required for this project, including those required by city, regional, state and federal agencies:

Additional Project Information

3. For non-residential projects, indicate total proposed building floor area: _____ sq. ft. in _____ floor(s).

4. Amount of off-street parking to be provided. _____ parking stalls. (Attach plans)

5. Proposed scheduling/development.

6. Associated project(s).

None

7. If residential, include the number of units, schedule of unit sizes, range of sale prices or rents, and type of household size expected. (This information will help the City track compliance with the objectives of the Housing Element of the General Plan.)

N/A

**CITY OF CORNING
PLANNING APPLICATION**

8. If commercial, indicate the type, whether neighborhood, city or regionally oriented, square footage of sales area, and loading facilities.

Truck wash, regional oriented
6,300 sq ft wash bays

9. If industrial, indicate type, estimated employment per shift, and loading facilities.

10. If institutional, indicate the primary function, estimated employment per shift, estimated occupancy, loading facilities, and community benefits to be derived from the project.

11. If the project involves a variance, conditional use permit or rezoning application, state this and indicate clearly why the application is required.

Are the following items applicable to the project or its effects? Discuss below all items checked yes (attach additional sheets as necessary).

- | | YES | NO |
|---|-------------------------------------|-------------------------------------|
| 12. Change in existing topographic features, or substantial alteration of ground contours? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| 13. Change in scenic views or vistas from existing residential areas or public lands or roads? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| 14. Change in pattern, scale or character of general area of project? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| 15. Significant amounts of solid waste or litter? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| 16. Change in dust, ash, smoke, fumes or odors in vicinity? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| 17. Change in lake, stream or ground water quality or quantity, or alteration of existing drainage patterns? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| 18. Substantial change in existing noise or vibration levels in the vicinity? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| 19. Is the site on filled land or on slopes of 10 percent or more? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| 20. Use, storage, or disposal of potentially hazardous materials, such as toxic substances, flammables or explosives? | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| 21. Substantial change in demand for municipal services (police, fire, water, sewage, etc.)? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| 22. Substantially increase energy usage (electricity, oil, natural gas, etc.)? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| 23. Relationship to a larger project or series of projects? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

**CITY OF CORNING
PLANNING APPLICATION**

Environmental setting

24. Describe the project site as it exists before the project, including information on topography, soil type and stability, plants and animals, and any cultural, historical or scenic aspects. Describe any existing structures on the site, and the use of the structures. Attach photographs of the site, snapshots or Polaroid photos will be accepted.

The existing site is a flat tract with no planted growth, no
wildlife and no apparent cultural, historical or scenic aspects.

There are no existing structures on the site.

25. Describe the surrounding properties, including information on plants and animals and any cultural, historical or scenic aspects. Indicate the type of land use (residential, commercial, etc.), intensity of land use (one-family, apartment houses, shops, department stores, etc.), and scale of development (height, frontage, set-back, rear yard, etc.). Attach photographs of the vicinity. Snapshots or Polaroid photos will be accepted.

South - very old motel/apartment and a truckwash/lube

operation. W - I-5

East - Old hyw 99

North - irrigation supply

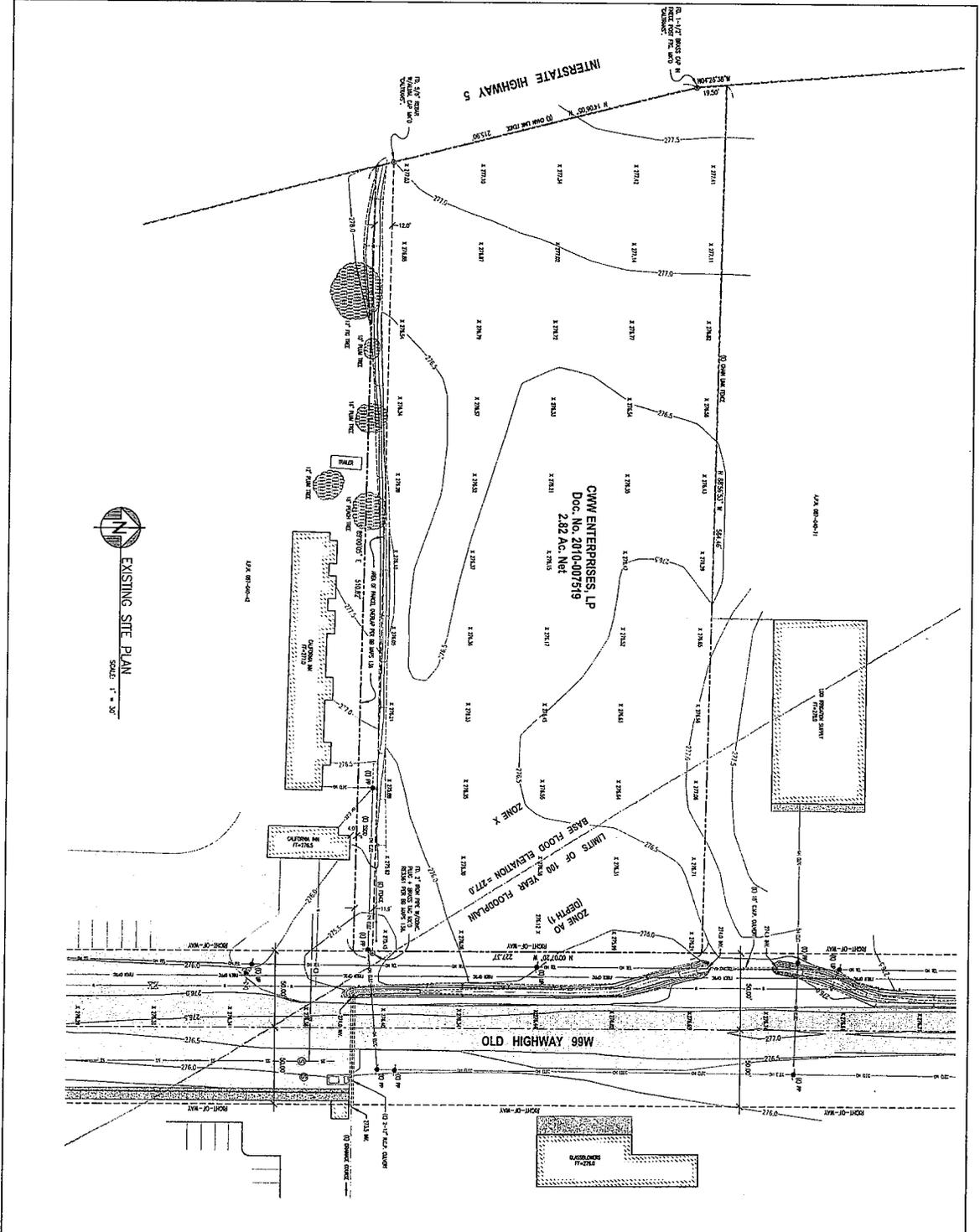
Certification

I hereby certify that the statements furnished above and in the attached exhibits present the data and information required for this initial evaluation to the best of my ability, and that the facts, statements, and information presented are true and correct to the best of my knowledge and belief.

Date 9-15-10

Signature Dana Moore

For: Blue Beacon of Corning





 EXISTING SITE PLAN

 SCALE: 1" = 30'

LEGEND:

- ▲ SLOPE CONTROL POINT
- SLOPE MARKER
- STORM DRAIN MANHOLE
- FIRE MANHOLE
- WATER VALVE
- POWER POLE
- STORM DRAIN INLET
- ASPHALT
- CONCRETE
- BUILDING
- DOWN LINE FENCE
- POWER POLE
- FLOW LINE
- UTILITY POLE
- TOP OF FINISHED
- TOP OF GRADED DRAIN
- INSIDE BOTTOM OF PIPE
- FINISH FLOOR
- FLOW LINE
- FINISH SURFACE
- DRAIN INLET
- WATER WALK
- EXISTING IMPROVEMENT

BENCHMARK:
 BENCHMARK: SURVEY STATION "A" 6" x 6" CONCRETE
 MANHOLE AT THE SOUTHWEST CORNER OF INTERSECTION
 AVENUE AND OLDEN AVENUE.
 ELEVATION = 343.89 (10/0 29)

BASIS OF BEARINGS:
 THE SOUTH LINE OF LOT 5, BLOCK 218 OF HANNOON
 COLONY 725 AS SHOWN ON THE RECORD OF SURVEY
 OFFICIAL RECORDS OF TULARE COUNTY, CALIFORNIA
 BEING SOUTH 89°00'00" EAST

SITE DATA:
 DESIGN PROJECT NO. 087-040-70
 SHEET NO. 10A (REVISED)

FLOOD ZONE:
 FLOOD ZONE "X" (GROUP 1) AND FLOOD ZONE "Y"
 (AREA DETERMINED TO BE OUTSIDE OF THE 100-YEAR
 FLOODPLAIN, BASE FLOOD ELEVATION DETERMINED BY
 LIMITS AS SHOWN ON F.E.A.V. FLOOD INSURANCE RATE
 EXISTING GROUND ELEVATION IN THE 1999-2003
 COASTAL PANEL NO. 060288 0000 (DATED 9/27/99)

UTILITY COMPANIES:
 CITY OF CORNING WATER & SEWER
 784 THIRD STREET
 CORNING, CA 93601 TEL: (530) 834-7028

S&B GAS AND ELECTRIC
 5600 W. 10TH STREET
 CORNING, CA 93602 TEL: (530) 834-7028

S&B TELEPHONE
 444 DIXON AVE
 830 BURLING, CA 93608 TEL: (530) 527-2834

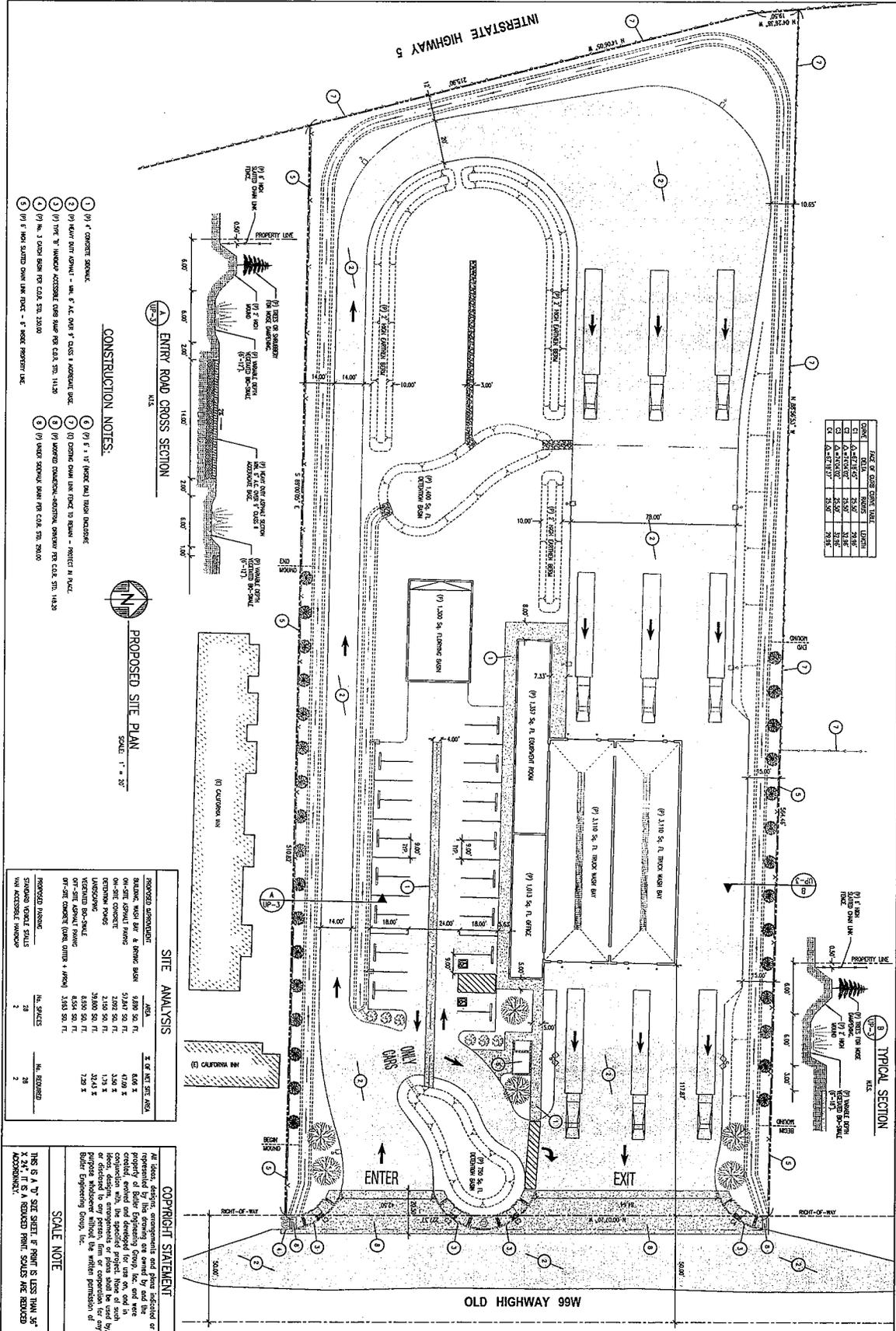
CONDUIT OWN
 TEL: (800) 288-5238

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 X 42", THIS IS A REDUCED PERM. SCALES ARE REVISED.
 10/04/10
 10.528

UP-2 <small>10/04/10</small> <small>10.528</small>	BLUE BEACON TRUCK WASH 3000 OLD HIGHWAY 99W A.P.N. 087-040-70 CORNING, CALIFORNIA EXISTING SITE PLAN	 butler ENGINEERING GROUP CIVIL • STRUCTURAL • SURVEYING <small>8837 AIRPORT RD. STE. A REDDING, CALIFORNIA 96002 T. 530.222.5311 • F. 530.226.3345 WWW.BUTLER-GRP.ORG</small>
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ITEM	DESCRIPTION	QUANTITY	UNIT
1	CONCRETE	15,000	CU YD
2	STEEL	100	TONS
3	ASPHALT	10,000	SQ YD
4	GRAVEL	20,000	CY
5	LANDSCAPING	10,000	SQ YD
6	PAVING	5,000	SQ YD
7	CONCRETE	5,000	CU YD
8	STEEL	50	TONS
9	ASPHALT	5,000	SQ YD
10	GRAVEL	10,000	CY
11	LANDSCAPING	5,000	SQ YD
12	PAVING	2,500	SQ YD
13	CONCRETE	2,500	CU YD
14	STEEL	25	TONS
15	ASPHALT	2,500	SQ YD
16	GRAVEL	5,000	CY
17	LANDSCAPING	2,500	SQ YD
18	PAVING	1,250	SQ YD
19	CONCRETE	1,250	CU YD
20	STEEL	12.5	TONS
21	ASPHALT	1,250	SQ YD
22	GRAVEL	2,500	CY
23	LANDSCAPING	1,250	SQ YD
24	PAVING	625	SQ YD
25	CONCRETE	625	CU YD
26	STEEL	6.25	TONS
27	ASPHALT	625	SQ YD
28	GRAVEL	1,250	CY
29	LANDSCAPING	625	SQ YD
30	PAVING	312.5	SQ YD
31	CONCRETE	312.5	CU YD
32	STEEL	3.125	TONS
33	ASPHALT	312.5	SQ YD
34	GRAVEL	625	CY
35	LANDSCAPING	312.5	SQ YD
36	PAVING	156.25	SQ YD
37	CONCRETE	156.25	CU YD
38	STEEL	1.5625	TONS
39	ASPHALT	156.25	SQ YD
40	GRAVEL	312.5	CY
41	LANDSCAPING	156.25	SQ YD
42	PAVING	78.125	SQ YD
43	CONCRETE	78.125	CU YD
44	STEEL	0.78125	TONS
45	ASPHALT	78.125	SQ YD
46	GRAVEL	156.25	CY
47	LANDSCAPING	78.125	SQ YD
48	PAVING	39.0625	SQ YD
49	CONCRETE	39.0625	CU YD
50	STEEL	0.390625	TONS
51	ASPHALT	39.0625	SQ YD
52	GRAVEL	78.125	CY
53	LANDSCAPING	39.0625	SQ YD
54	PAVING	19.53125	SQ YD
55	CONCRETE	19.53125	CU YD
56	STEEL	0.1953125	TONS
57	ASPHALT	19.53125	SQ YD
58	GRAVEL	39.0625	CY
59	LANDSCAPING	19.53125	SQ YD
60	PAVING	9.765625	SQ YD
61	CONCRETE	9.765625	CU YD
62	STEEL	0.09765625	TONS
63	ASPHALT	9.765625	SQ YD
64	GRAVEL	19.53125	CY
65	LANDSCAPING	9.765625	SQ YD
66	PAVING	4.8828125	SQ YD
67	CONCRETE	4.8828125	CU YD
68	STEEL	0.048828125	TONS
69	ASPHALT	4.8828125	SQ YD
70	GRAVEL	9.765625	CY
71	LANDSCAPING	4.8828125	SQ YD
72	PAVING	2.44140625	SQ YD
73	CONCRETE	2.44140625	CU YD
74	STEEL	0.0244140625	TONS
75	ASPHALT	2.44140625	SQ YD
76	GRAVEL	4.8828125	CY
77	LANDSCAPING	2.44140625	SQ YD
78	PAVING	1.220703125	SQ YD
79	CONCRETE	1.220703125	CU YD
80	STEEL	0.01220703125	TONS
81	ASPHALT	1.220703125	SQ YD
82	GRAVEL	2.44140625	CY
83	LANDSCAPING	1.220703125	SQ YD
84	PAVING	0.6103515625	SQ YD
85	CONCRETE	0.6103515625	CU YD
86	STEEL	0.006103515625	TONS
87	ASPHALT	0.6103515625	SQ YD
88	GRAVEL	1.220703125	CY
89	LANDSCAPING	0.6103515625	SQ YD
90	PAVING	0.30517578125	SQ YD
91	CONCRETE	0.30517578125	CU YD
92	STEEL	0.0030517578125	TONS
93	ASPHALT	0.30517578125	SQ YD
94	GRAVEL	0.6103515625	CY
95	LANDSCAPING	0.30517578125	SQ YD
96	PAVING	0.152587890625	SQ YD
97	CONCRETE	0.152587890625	CU YD
98	STEEL	0.00152587890625	TONS
99	ASPHALT	0.152587890625	SQ YD
100	GRAVEL	0.30517578125	CY
101	LANDSCAPING	0.152587890625	SQ YD



- CONSTRUCTION NOTES:**
- 1) 4" CONCRETE SLAB
 - 2) 1/2" REBAR ON 12" SPACING - MIN. 6" A.C. OVER 5" CLASS 1 ASPHALT PAD
 - 3) 1/2" REBAR ON 12" SPACING - MIN. 6" A.C. OVER 5" CLASS 1 ASPHALT PAD
 - 4) 1/2" REBAR ON 12" SPACING - MIN. 6" A.C. OVER 5" CLASS 1 ASPHALT PAD
 - 5) 1/2" REBAR ON 12" SPACING - MIN. 6" A.C. OVER 5" CLASS 1 ASPHALT PAD

PROPOSED SITE PLAN
SCALE: 1" = 30'

SITE ANALYSIS

PROPOSED DEVELOPMENT	AREA	% OF NET SITE AREA
SOILS	5,985 SQ. FT.	6.6%
ASPHALT	2,100 SQ. FT.	2.3%
CONCRETE	2,100 SQ. FT.	2.3%
PAVING	2,100 SQ. FT.	2.3%
LANDSCAPING	79,800 SQ. FT.	87.5%
UNDEVELOPED	8,595 SQ. FT.	9.5%
GRAVEL	8,595 SQ. FT.	9.5%
ASPHALT	1,191 SQ. FT.	1.3%
CONCRETE	1,191 SQ. FT.	1.3%
PAVING	1,191 SQ. FT.	1.3%
LANDSCAPING	1,191 SQ. FT.	1.3%
UNDEVELOPED	1,191 SQ. FT.	1.3%

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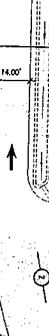
REVISIONS

NO.	DATE	DESCRIPTION
1	10/04/10	ISSUED FOR USE PERMIT - NOT FOR CONSTRUCTION

UP-3
10/04/10
10-328

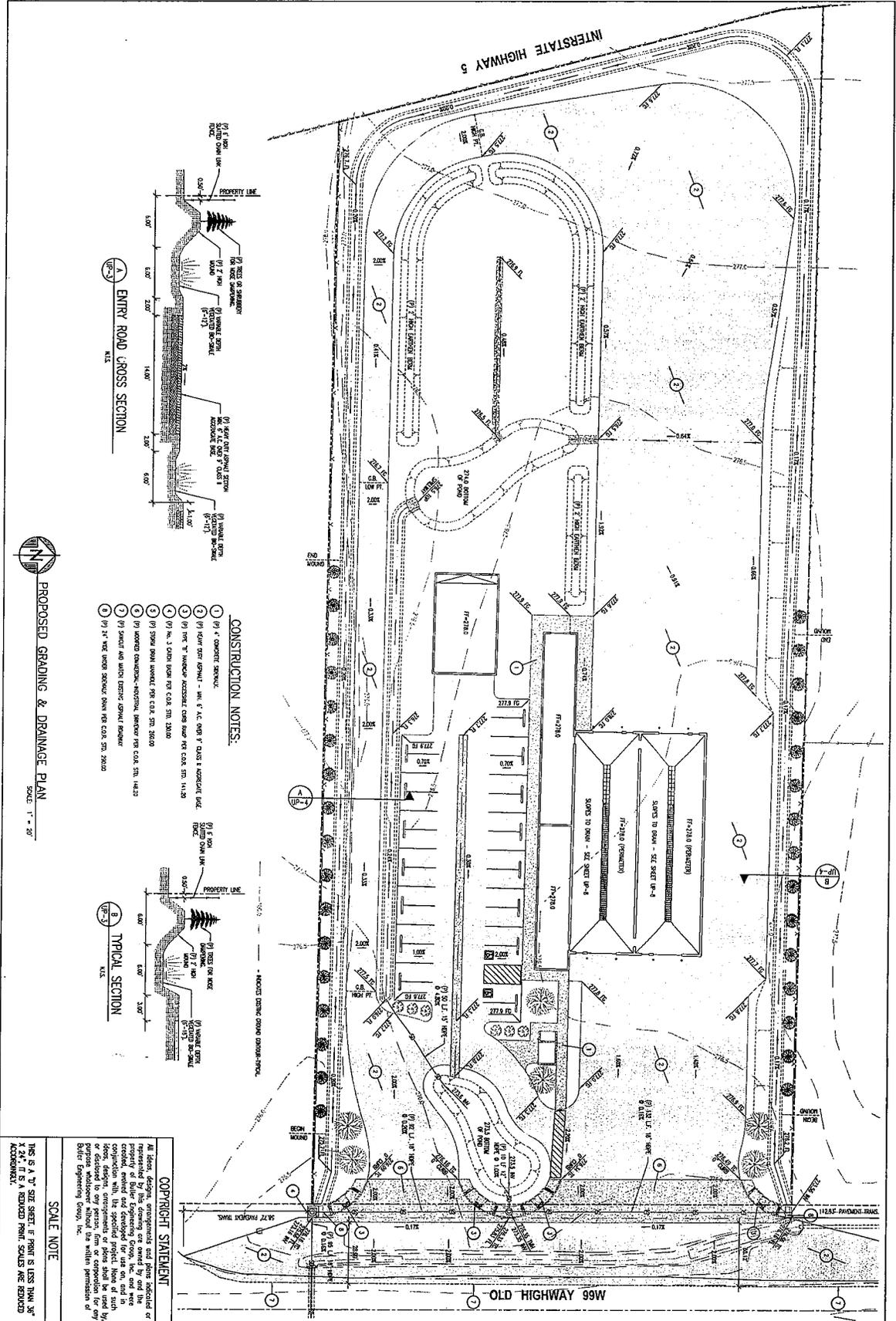
BLUE BEACON TRUCK WASH
3000 OLD HIGHWAY 99W
A.P.N. 087-040-70
CORNING, CALIFORNIA

PROPOSED SITE PLAN

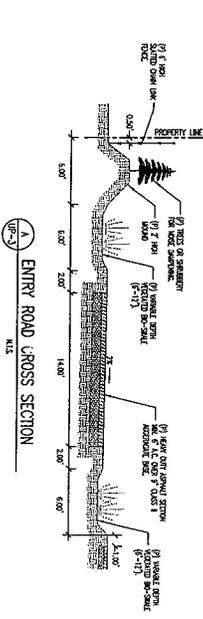


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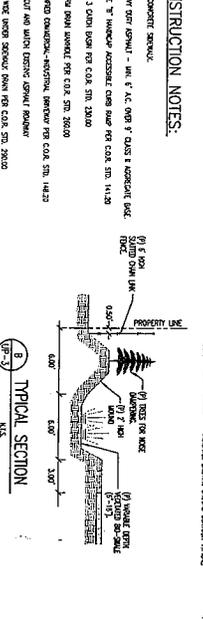
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ENTRY ROAD CROSS SECTION



TYPICAL SECTION



- CONSTRUCTION NOTES:**
- (1) 1" x 1" CONCRETE SIDEWALK
 - (2) 12" WIDE GRAVEL SHOULDER - MIN. 6" A.C. OVER 8" CURB & ASPHALT PAD.
 - (3) 12" WIDE GRAVEL SHOULDER - MIN. 6" A.C. OVER 8" CURB & ASPHALT PAD.
 - (4) 12" WIDE GRAVEL SHOULDER - MIN. 6" A.C. OVER 8" CURB & ASPHALT PAD.
 - (5) 12" WIDE GRAVEL SHOULDER - MIN. 6" A.C. OVER 8" CURB & ASPHALT PAD.
 - (6) 12" WIDE GRAVEL SHOULDER - MIN. 6" A.C. OVER 8" CURB & ASPHALT PAD.
 - (7) 12" WIDE GRAVEL SHOULDER - MIN. 6" A.C. OVER 8" CURB & ASPHALT PAD.
 - (8) 12" WIDE GRAVEL SHOULDER - MIN. 6" A.C. OVER 8" CURB & ASPHALT PAD.

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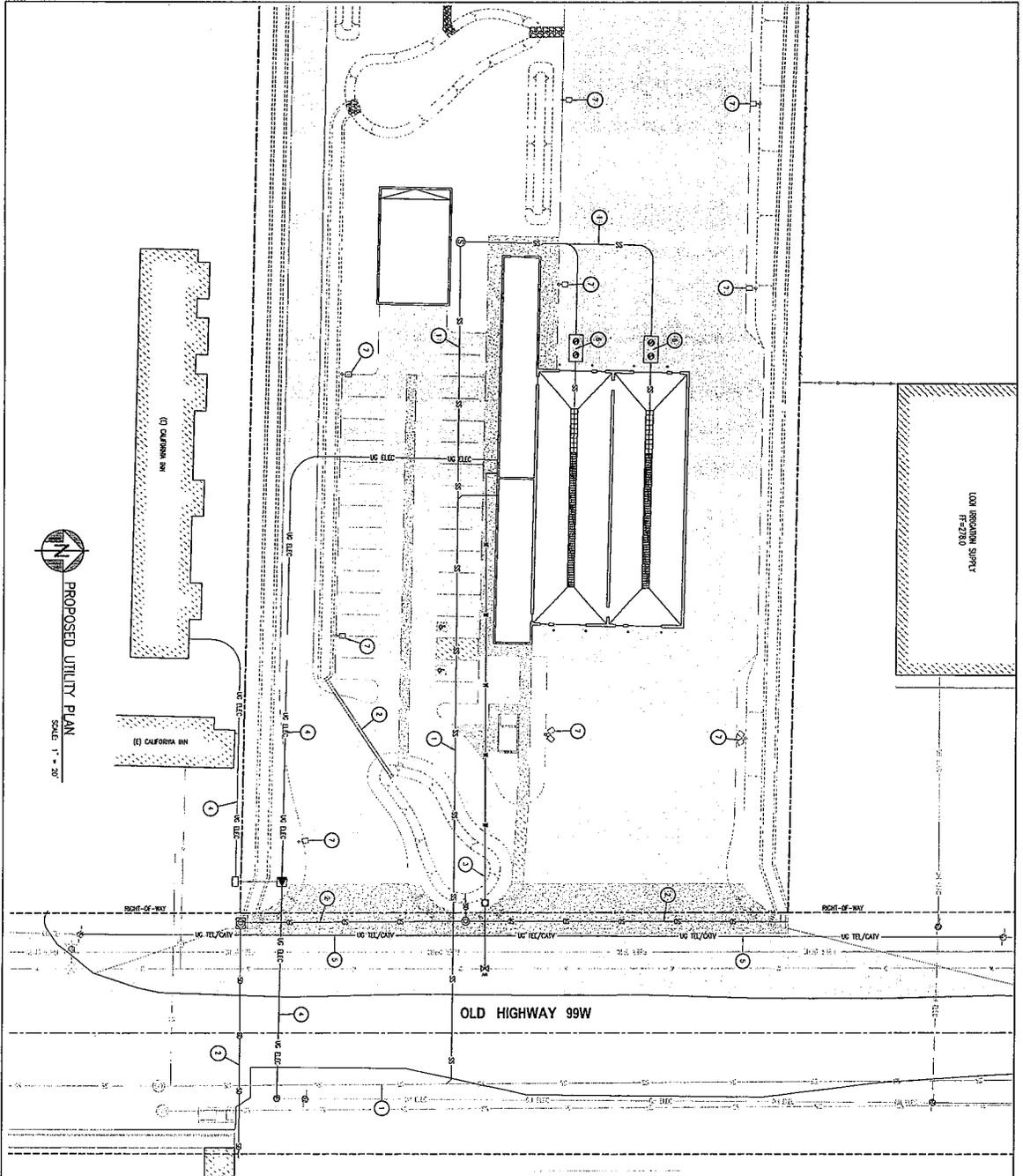
NO.	DATE	REVISIONS
1	10/04/10	ISSUED FOR USE PERMIT-NOT FOR CONSTRUCTION

BLUE BEACON TRUCK WASH
 3000 OLD HIGHWAY 99W
 A.P.N. 087-040-70
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UP4
 10/04/10
 10/26/10





 PROPOSED UTILITY PLAN

 SCALE: 1" = 20'

CONSTRUCTION NOTES:

- ① PROPOSED 6" PVC SANITARY SOAK
- ② PROPOSED 8" WATER MAIN - SEE PROPOSED DRAWING A. REMOVE MAIN W-4
- ③ PROPOSED 4" WATER SERVICE
- ④ PROPOSED UNDERGROUND ELECTRICAL
- ⑤ PROPOSED UNDERGROUND TELEPHONE/CABLE
- ⑥ SHED & OFFICE RELOCATIONS
- ⑦ SHED SHIFTS - SEE PROPOSED SHED LOCATIONS SHEET W-4

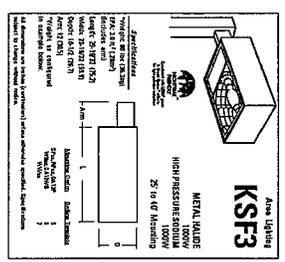
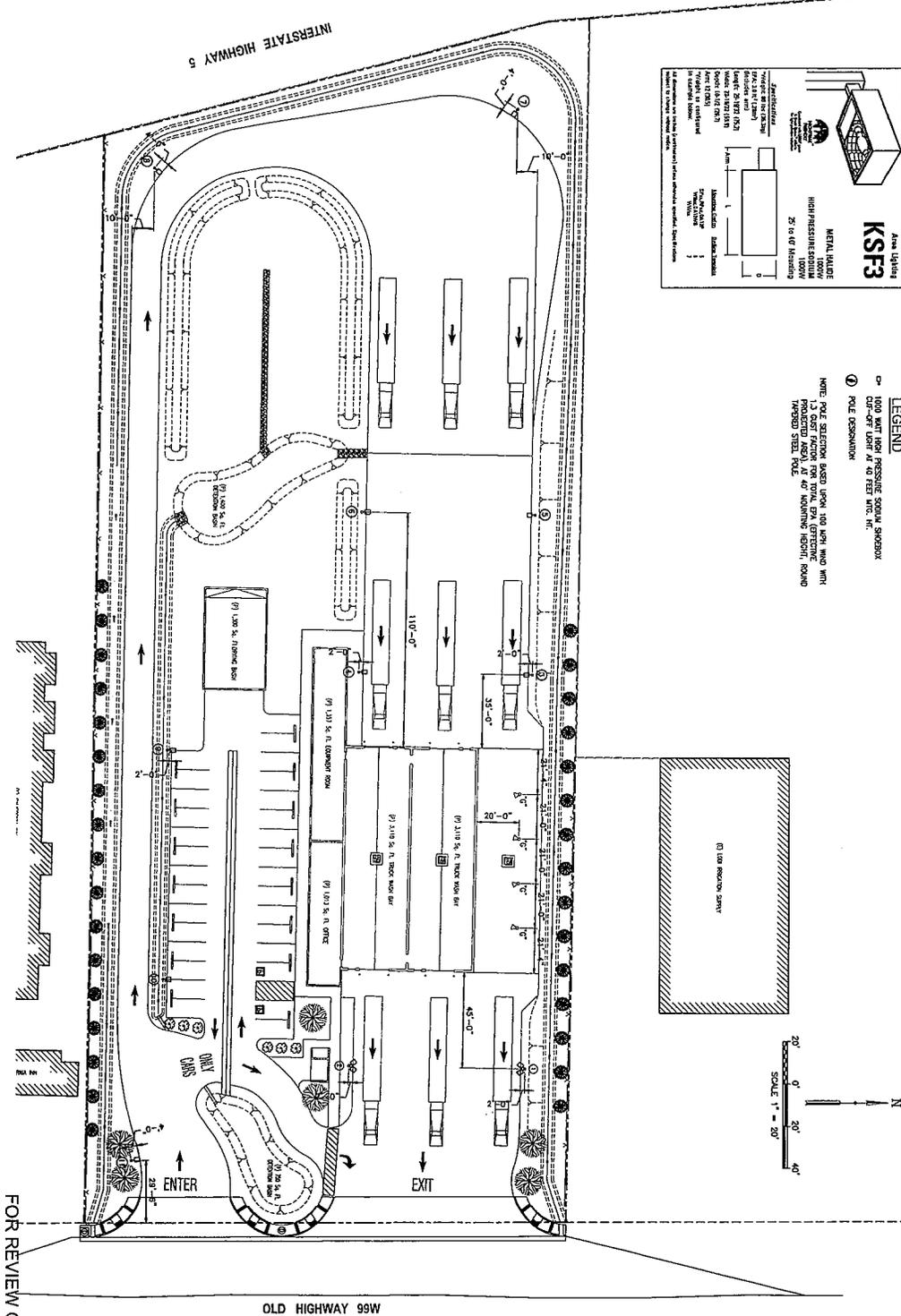
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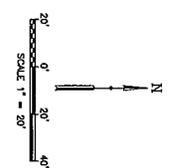
 10/23/08	BLUE BEACON TRUCK WASH 3000 OLD HIGHWAY 99W A.P.N. 087-040-70 CORNING, CALIFORNIA PROPOSED UTILITY PLAN	 8837 AIRPORT RD. STE. A REDDING, CALIFORNIA 96002 T. 530.222.9211 • F. 530.228.345 WWW.BUTLER-GROUP.ORG	<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NO.</th> <th>DATE</th> <th>REVISIONS</th> </tr> </thead> <tbody> <tr> <td>1</td> <td>10/04/10</td> <td>ISSUED FOR USE PERMIT-NOT FOR CONSTRUCTION.</td> </tr> <tr> <td> </td> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>	NO.	DATE	REVISIONS	1	10/04/10	ISSUED FOR USE PERMIT-NOT FOR CONSTRUCTION.						
NO.	DATE	REVISIONS													
1	10/04/10	ISSUED FOR USE PERMIT-NOT FOR CONSTRUCTION.													



LEGEND

- 1000 WATT HIGH PRESSURE SODIUM SPOUX
- OFF-OFF LIGHT AT 40 FEET WTS. Ht.
- ① POLE DESIGNATION

NOTE: 1.3 FOOT FACTOR FOR TOTAL SPA EFFECTIVE HEIGHT AND USE OF 40' MOUNTING HEIGHT, ROUND PIPING SHALL BE USED.



FOR REVIEW ONLY

UP-6

DATE: 10/10/10

PRELIMINARY REVIEW

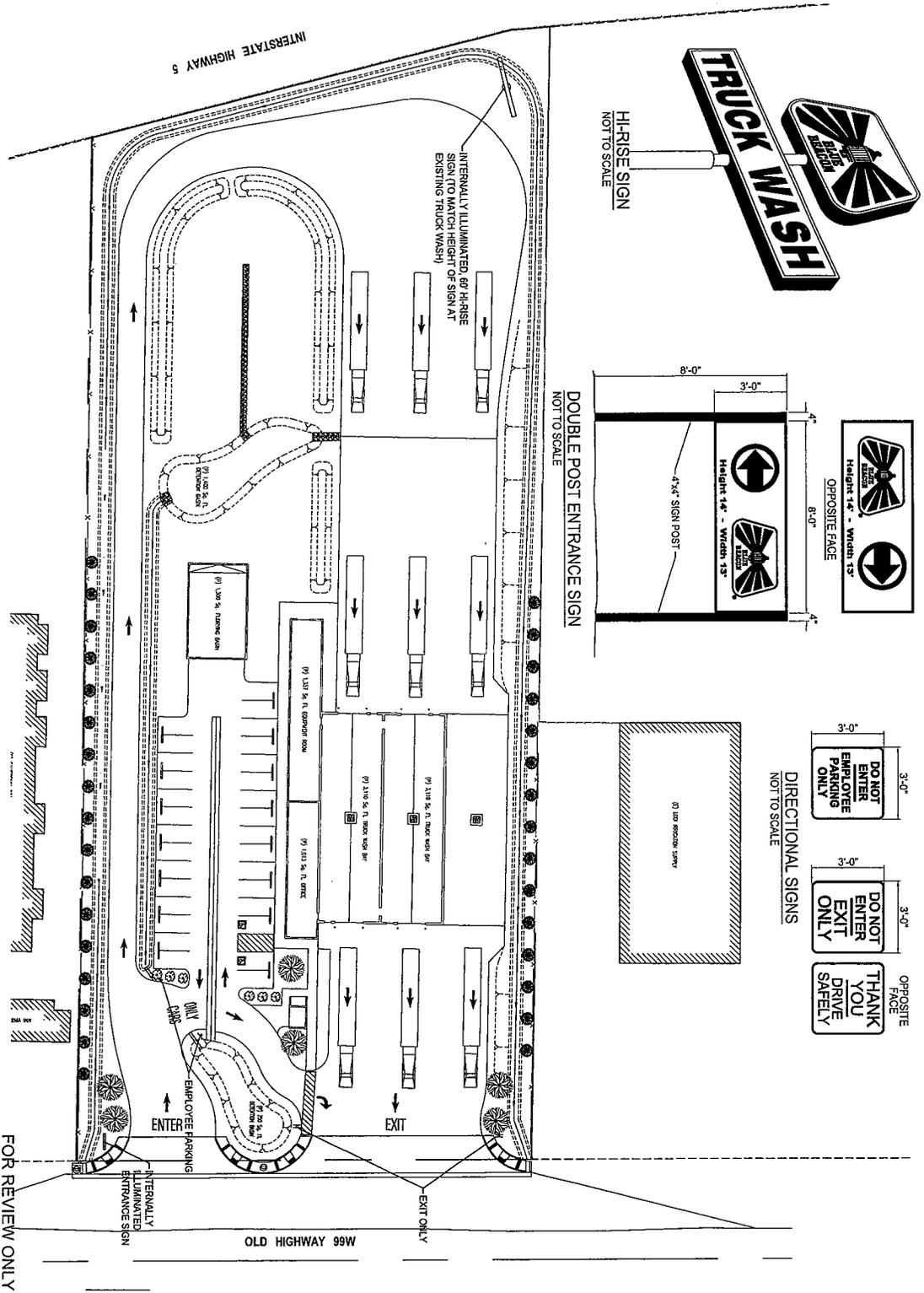
SITE LIGHTING LAYOUT

BLUE BEACON OF CORNING, CA

OLD HIGHWAY 99W - CORNING CALIFORNIA

PLANS PROVIDED BY

BLUE BEACON INTERNATIONAL INC.

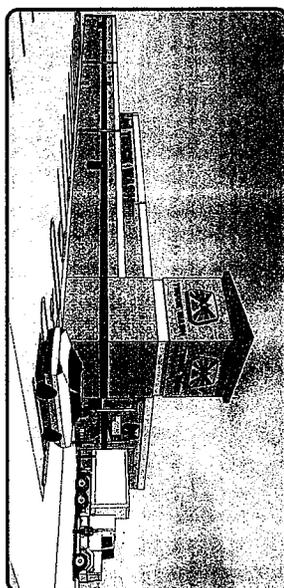
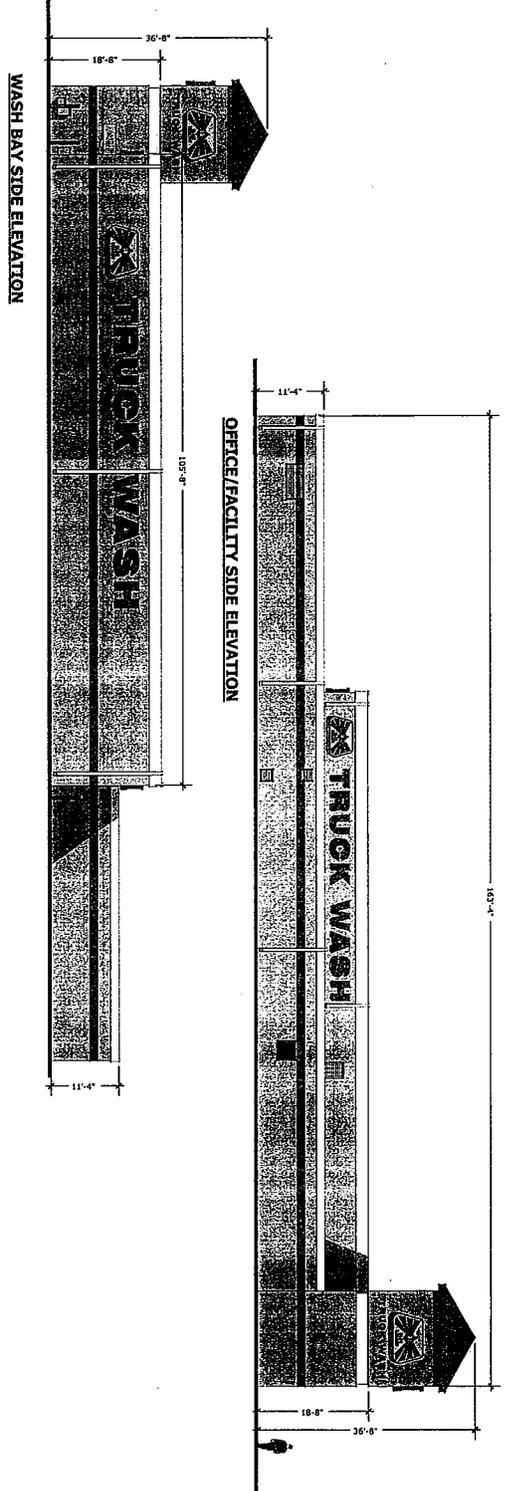


SHEET NO. 100
 PRELIMINARY REVIEW
 DATE: 10-01-2010
 PROJECT: SITE SIGNING LAYOUT
 CLIENT: BLUE BEACON OF CORNING, CA
 LOCATION: OLD HIGHWAY 99W - CORNING CALIFORNIA

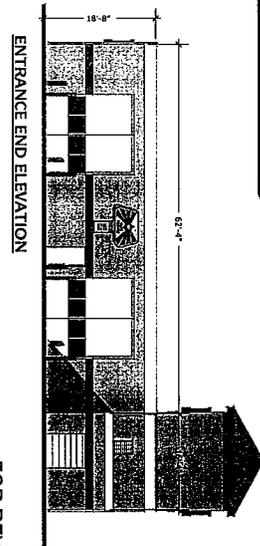
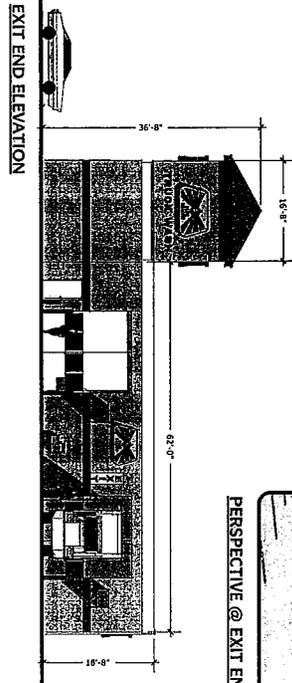
DRAWN BY: [Name]
 CHECKED BY: [Name]
 DATE: [Date]

SCALE: 1" = 10'-0"
 DATE: 10-01-2010

BLUE BEACON INTERNATIONAL INC.
 1000 [Address]
 CORNING, CA 91301
 TEL: [Phone Number]
 FAX: [Phone Number]
 WWW.BLUEBEACON.COM



PERSPECTIVE @ EXIT END



FOR REVIEW ONLY

UP-10 DATE: 10/01/10	SUBMITTAL: PRELIMINARY REVIEW DATE: 10-01-2010	SHEET TAG: COLOR ELEVATIONS BLUE BEACON OF CORNING, CA OLD HIGHWAY 99W - CORNING CALIFORNIA	BLUE BEACON INTERNATIONAL INC.
	TITLE: PROJECT: DRAWN BY: CHECKED BY: APPROVED BY:	DESIGNED BY: DATE:	

CITY OF CORNING PLANNING DEPARTMENT
794 THIRD STREET
CORNING, CALIFORNIA 96021

MITIGATED NEGATIVE DECLARATION
FOR THE USE PERMIT 2010-160

PROJECT TITLE: Use Permit 2010-160, Blue Beacon, International

DESCRIPTION OF PROJECT: Construct a new truck wash facility with two 3,110 sq. ft. wash bays, a 1,357 sq. ft. equipment room, and 1,013 sq. ft. office space area. Additional site development will include asphalt drives for entering and exiting the site and wash bays along with an employee and customer car parking lot, a 1,300 sq. ft. drying basin, landscape berms along the north and south property lines, and two detention ponds. This facility will replace an existing truck wash operated by Blue Beacon International, Inc. located along the south side of South Ave. approximately 1000 feet east of the South Ave. / Hwy 99W intersection. The proposed location for the new truck wash is on the west side of Hwy 99W, approximately 800 feet north of the South Ave. / Hwy 99W intersection. APN: 87-040-70

The City of Corning Planning Department has evaluated potential environmental impacts and prepared an Initial Study, using the Initial Study Environmental Checklist Form distributed by the California Office of Planning and Research, and found that with the implementation of mitigation measures and recommended conditions of approval, identified in the initial study, the above described project will have no significant adverse effect on the environment.

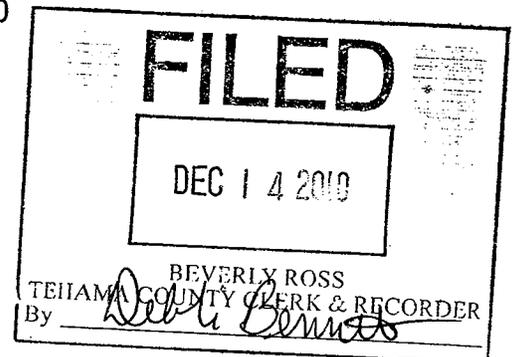
Attached is a copy of the Initial Study with identified mitigations and recommended conditions of approval, a Mitigation Monitoring Program and a Traffic Study that is included as an addendum to the Initial Study. Copies of this Mitigated Negative Declaration and Initial Study are available upon request from the Tehama County Clerk & Recorder's Office and the City of Corning Planning Department.

Those wishing to comment regarding this Mitigated Negative Declaration must do so on or before **January 16, 2011**. Comments received after this date will not be valid.

2011/3


John Stouffer :
Planning Director

DATE: 12-14-10



CITY OF CORNING PLANNING DEPARTMENT
ENVIRONMENTAL SIGNIFICANCE CHECKLIST
Meets requirements of CEQA §15063(d), Initial Study

BACKGROUND

1. PROJECT TITLE: Use Permit 2010-260, Blue Beacon International, Inc.
2. LEAD AGENCY NAME AND ADDRESS: City of Corning Planning Department
794 Third Street
Corning, CA 96021
(530) 824-7036, FAX: (530) 824-2489
jstoufer@corning.org
3. CONTACT PERSON: John Stoufer, Planning Director
4. APPLICANT/PROJECT PROPONENT NAME AND ADDRESS: Blue Beacon International, Inc.
500 Graves Blvd
Salina, KS 67401
5. DESCRIPTION OF PROJECT: Construct a new truck wash facility with two 3,110 sq. ft. wash bays, a 1,357 sq. ft. equipment room, and 1,013 sq. ft. office space area. Additional site development will include asphalt drives for entering and exiting the site and wash bays along with an employee and customer car parking lot, a 1,300 sq. ft. drying basin, landscape berms along the north and south property lines, and two detention ponds. This facility will replace an existing truck wash operated by Blue Beacon International, Inc. located along the south side of South Ave. approximately 1000 feet east of the South Ave. / Hwy 99W intersection.
6. PROJECT LOCATION: In the N1/2 of Section 28, T. 24N., R. 3W., MDM, along the west side of Hwy 99W, approximately 800 feet north of the South Ave. / Hwy 99W intersection.
7. GENERAL PLAN DESIGNATION: Hwy 99-W Specific Plan
8. ZONING: CH-CBDZ, Highway Service Commercial – Corning Business Development Zone
9. SETTING AND SURROUNDING LAND USES: The site is vacant, flat, and sparsely covered with annual grasses. West of the site is Interstate 5 and the South Ave. interchange. South of the site is a small motel known as the California Inn, another truck wash, a fast food restaurant and a small liquor store. East of the site is a vacant commercial building known as the Glassblowers Building and a Loves truck stop. North of the site is a wholesale agricultural irrigation business known as Lodi Irrigation and then vacant land covered with annual grasses.
10. CONSISTENCY WITH GENERAL PLAN AND ZONING: Establishment of a commercial truck wash business is consistent with commercial uses permitted in the Hwy 99-W Specific Plan and permitting the use pursuant to the issuance of a conditional use permit is consistent with the CH-CBDZ, Highway Service Commercial – Corning Business Development Zone, Zoning District.

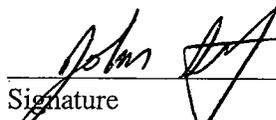
ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

The environmental factors identified below could be potentially affected by this project:

X	AESTHETICS		AGRICULTURAL AND FORESTRY RESOURCES	X	AIR QUALITY
	BIOLOGICAL RESOURCES	X	CULTURAL RESOURCES	X	GEOLOGY AND SOILS
X	GREENHOUSE GAS EMISSIONS	X	HAZARDS AND HAZARDOUS MATERIALS		HYDROLOGY AND WATER QUALITY
	LAND USE AND PLANNING		MINERAL RESOURCES	X	NOISE
	POPULATION AND HOUSING		PUBLIC SERVICES		RECREATION
	TRANSPORTATION/ TRAFFIC		UTILITY AND SERVICE SYSTEMS		MANDATORY FINDINGS OF SIGNIFICANCE
	NONE				

DETERMINATION: (To be completed by the Lead Agency) On the basis of this initial evaluation:

	I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
	I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
	I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
	I find that the proposed project MAY have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
	I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.


 Signature
 John Stoufer, Planning Director

12/14/10
 Date

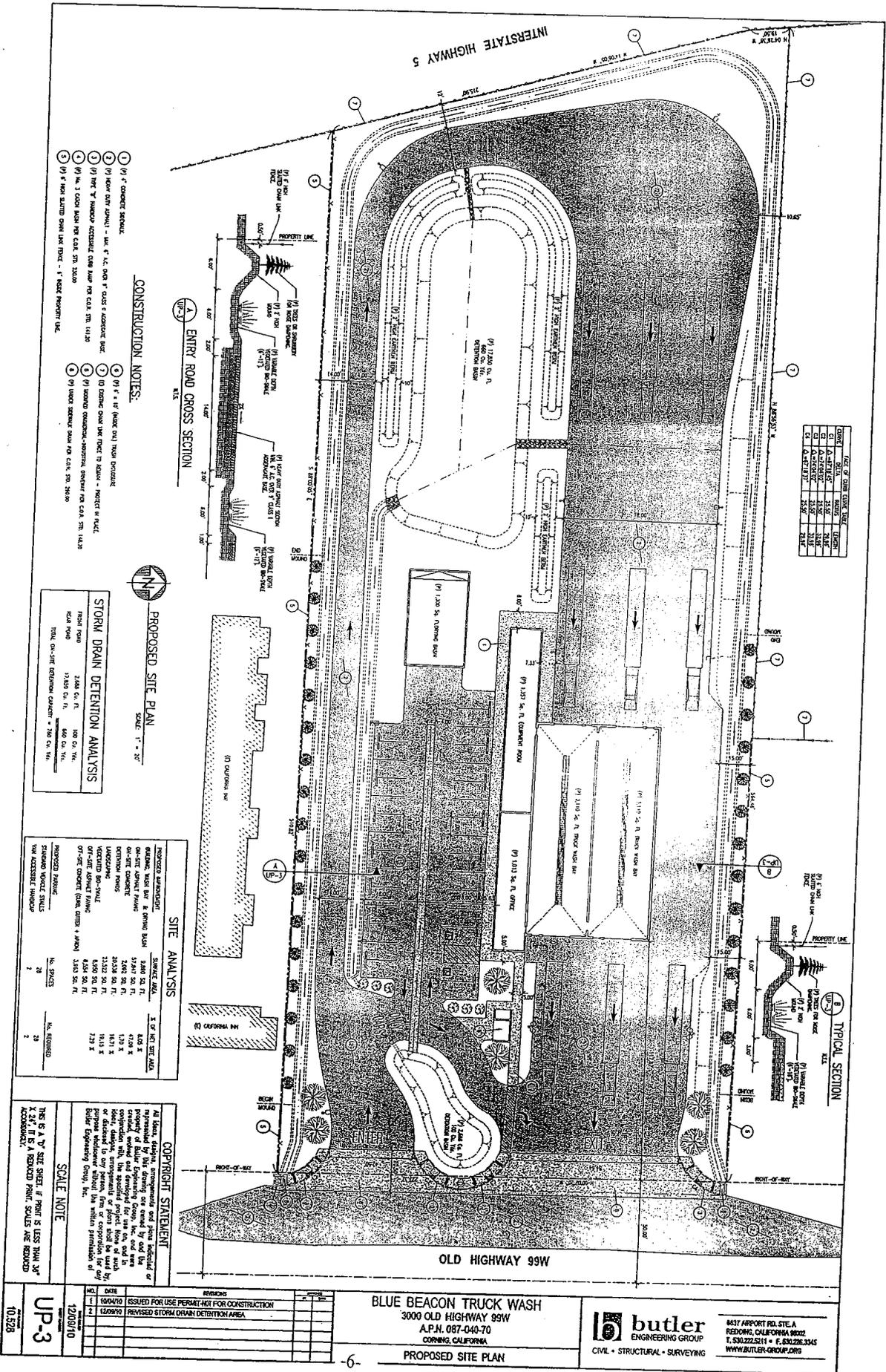
EVALUATION OF ENVIRONMENTAL IMPACTS:

- 1) A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g., the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g., the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).
- 2) All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
- 3) Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect may be significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.
- 4) "Negative Declaration: Less Than Significant With Mitigation Incorporated" applies where the incorporation of mitigation measures has reduced an effect from "Potentially Significant Impact" to a "Less Than Significant Impact." The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from Section XVII, "Earlier Analyses," may be cross-referenced).
- 5) Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. Section 15063(c)(3)(D). In this case, a brief discussion should identify the following:
 - a) Earlier Analysis Used. Identify and state where they are available for review.
 - b) Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
 - c) Mitigation Measures. For effects that are "Less than Significant with Mitigation Measures Incorporated," describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.
- 6) Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g., general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.

- 7) Supporting Information Sources: A source list should be attached, and other sources used or individuals contacted should be cited in the discussion.
- 8) This is only a suggested form, and lead agencies are free to use different formats; however, lead agencies should normally address the questions from this checklist that are relevant to a project's environmental effects in whatever format is selected.
- 9) The explanation of each issue should identify:
 - a) the significance criteria or threshold, if any, used to evaluate each question; and
 - b) the mitigation measure identified, if any, to reduce the impact to less than significance

EXHIBITS

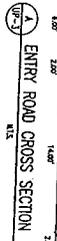
- 1. Site Plan** **Page 6**
- 2. Building Elevation Plan** **Page 7**



LIST OF QUANTITIES	
NO.	DESCRIPTION
1	CONCRETE
2	STEEL
3	PAVING
4	LANDSCAPING
5	UTILITIES
6	MECHANICAL
7	ELECTRICAL
8	PLUMBING
9	FINISHES
10	OTHER

CONSTRUCTION NOTES:

- 1) 2" CONCRETE SLAB.
- 2) 4" CONCRETE SLAB.
- 3) 6" CONCRETE SLAB.
- 4) 8" CONCRETE SLAB.
- 5) 10" CONCRETE SLAB.
- 6) 12" CONCRETE SLAB.
- 7) 14" CONCRETE SLAB.
- 8) 16" CONCRETE SLAB.
- 9) 18" CONCRETE SLAB.
- 10) 20" CONCRETE SLAB.
- 11) 22" CONCRETE SLAB.
- 12) 24" CONCRETE SLAB.
- 13) 26" CONCRETE SLAB.
- 14) 28" CONCRETE SLAB.
- 15) 30" CONCRETE SLAB.



STORM DRAIN DETENTION ANALYSIS

Peak Flow	2,888 CFS
Peak Time	13.885 Min.
Volume	408,000 Gallons
Detention Capacity	1,000,000 Gallons

SITE ANALYSIS

PROPOSED IMPROVEMENT	NO. SQUARE FEET	NO. SQUARE FEET
REMOVED IMPROVEMENT <td>1,000</td> <td>1,000</td>	1,000	1,000
ADDED IMPROVEMENT <td>1,000</td> <td>1,000</td>	1,000	1,000
NET IMPROVEMENT <td>0</td> <td>0</td>	0	0

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SCALE NOTE

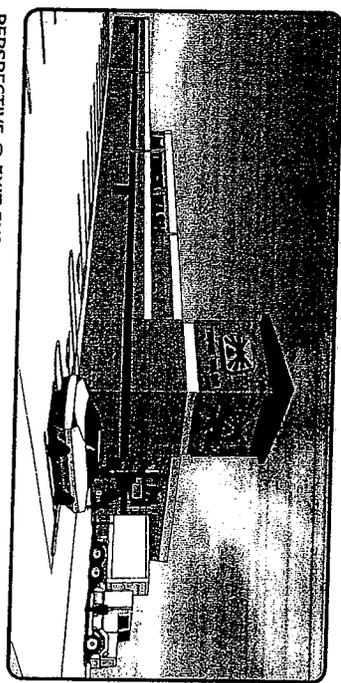
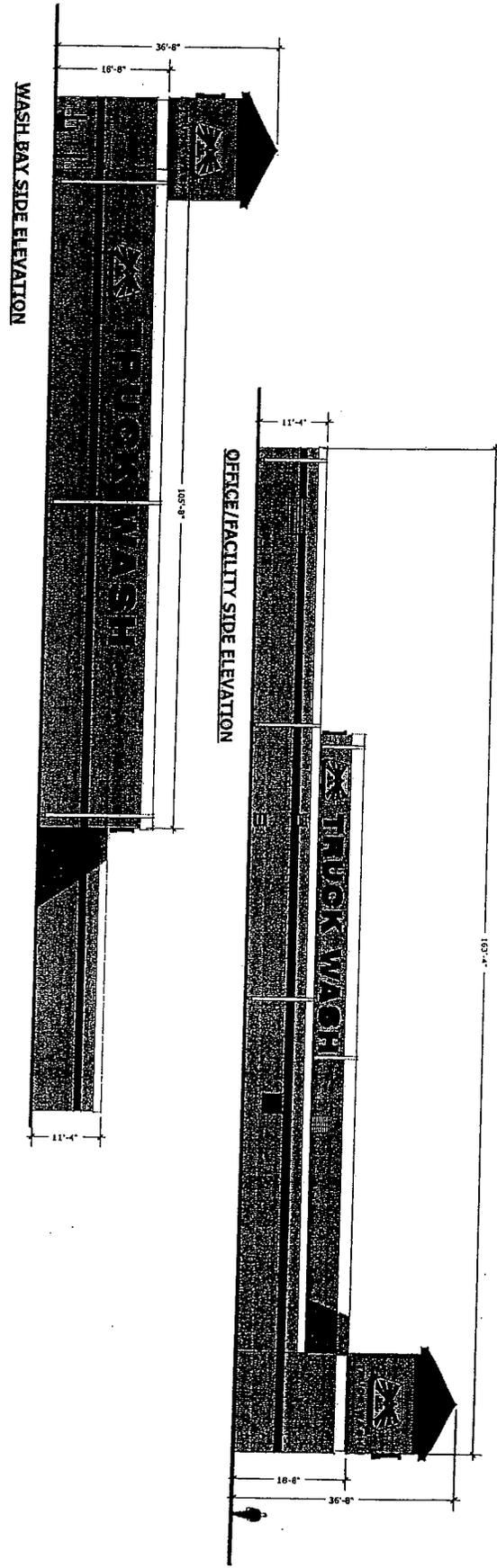
THIS IS A 1/4" SIZE SHEET. IF PRINT IS LESS THAN 1/4" X 24", IT IS A REDUCED PRINT. SCALES ARE REDUCED ACCORDINGLY.

NO.	DATE	REVISION
1	10/04/20	ISSUED FOR USE PERMIT-ACT FOR CONSTRUCTION
2	12/09/20	REVISED STORM DRAIN DETENTION AREA

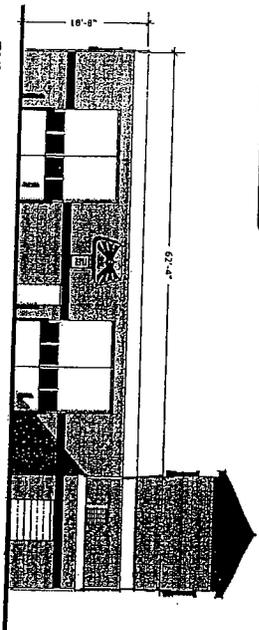
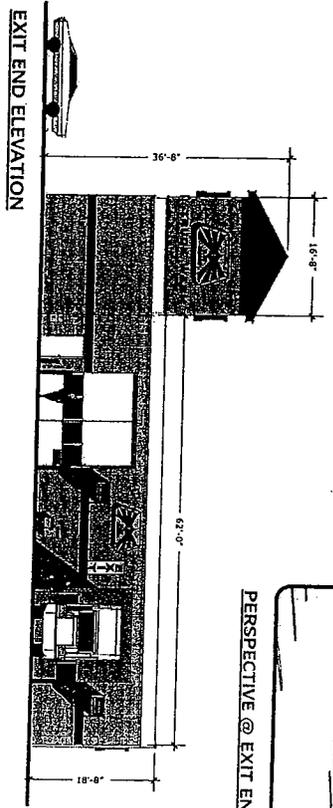
BLUE BEACON TRUCK WASH
 3000 OLD HIGHWAY 99W
 A.P.N. 087-040-70
 CORNING, CALIFORNIA

butler
 ENGINEERING GROUP
 CIVIL • STRUCTURAL • SURVEYING

8437 AIRPORT RD. STE. A
 REDDING, CALIFORNIA 96002
 T. 530.222.5211 • F. 530.228.3345
 WWW.BUTLER-GROUP.COM



PERSPECTIVE @ EXIT END



FOR REVIEW ONLY

UP-10 SHEET NO. OF 10	PRELIMINARY REVIEW DATE: 10-01-2010	COLOR ELEVATIONS BLUE BEACON OF CORNING, CA OLD HIGHWAY BENT - CORNING CALIFORNIA	BLUE BEACON INTERNATIONAL INC.
	-7-		

EARLIER ANALYSES.

Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, one or more effects have been adequately analyzed in an earlier EIR or negative declaration. [Section 15603(C)(3)(D)].

 X NO EARLIER ANALYSIS USED.

In this case, a discussion of issues will identify the following:

- a) **Earlier analyses used.** Identify earlier analyses and state where they are available for review.
- b) **Impacts adequately addressed.** Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
- c) **Mitigation measures.** For effects that are *Negative Declarations: Less than Significant with Mitigation Incorporated*, describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project. *Attach original mitigation measures.*

<i>Issues:</i>	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
I. AESTHETICS -- Would the project:				
a) Have a substantial adverse effect on a scenic vista?				X
b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?				X
c) Substantially degrade the existing visual character or quality of the site and its surroundings?		X		
d) Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?			X	

DISCUSSION: The site is relatively flat with no structures, scenic vistas or scenic resources that will be impacted by development of the site. Just south of the site is the South Ave. interchange that is developed with a variety of commercial business geared toward the fueling and servicing of trucks and trailers that travel on Interstate 5. These businesses have lights for 24 hour service. Additional lighting at this site, which is adjacent to these existing businesses, will not create a new source of light or glare that will affect day or nighttime views in the area.

The site is located within the Highway 99W Corridor Specific Plan. This specific plan was adopted to provide a more detailed examination of the planning issues in the corridor than could be achieved in the City's General Plan. Specific plans offer a unique opportunity to establish a comprehensive approach to planning and development issues by integrating community goals and policies and combining zoning regulations, capital improvement programs, detailed site development standards, and other regulatory tools into one document tailored to meet the needs of a particular area.

The specific plan contains a community design element that provides visual design guidelines that were adopted to promote thoughtful and responsible design which is consistent with the City's character. The specific plan states "Landscaping is a major factor in creating a positive image of an area. The landscaping of a new project should attempt to do more than just make a place look attractive. Landscaping themes can be used to enhance site development and promote the continuity between developments. Plants can perform a number of functions to enhance the land use, screen heat and glare, provide buffers, emphasize entrances and exits, and soften the lines of architecture and paving."

The specific plan provides sign design guidelines in an attempt to safeguard life, health, property and public welfare, and to preserve the character of the City by regulating the size, height, design, quality of materials, construction, location, lighting and maintenance of all signs not enclosed within a building. To assure that development of the site as a truck wash complies with the landscaping and sign requirements of the specific plan and at the same time reduces the visual impacts to a Less than Significant level the following mitigation measure will be implemented:

Mitigation Measure I. C. 1

LANDSCAPING PLANS. Prior to commencing construction activities associated with the development of the site as a truck wash, the applicant or his engineer shall submit landscaping and signage plans that comply with the landscaping design guidelines and sign design guidelines of the Highway 99W Corridor Specific Plan. The landscaping plans must include irrigation plans that comply with the “Water Efficient Landscape Regulations” pursuant to Section 15.08.055 of the Corning Municipal Code. Both plans must be approved by city staff prior to installation and the sign installation and landscaping must be completed prior to the city issuing a certificate of occupancy for the truck wash buildings.

CONCLUSION: The signage and landscaping requirements set out in the Hwy. 99W Corridor Specific Plan and complying with the following conditions of approval will reduce the visual impacts associated with development of the site as a truck wash to a Less than Significant Level.

RECOMMENDED CONDITIONS OF APPROVAL:

UNDERGROUND UTILITIES. Underground existing telephone and CATV along the Highway 99-W frontage and remove existing utility pole as shown on the Improvement Plans, Utility Plan Sheet UP5, submitted with the Use Permit application.

REMOVE CONSTRUCTION DEBRIS. Prior to the issuance of a Certificate of Occupancy for the truck wash buildings all construction debris must be removed from the site.

<i>Issues:</i>	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
<p>II. AGRICULTURE AND FOREST RESOURCES: In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Dept. of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board. -- Would the project:</p>				
<p>a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to nonagricultural use?</p>				X
<p>b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?</p>				X
<p>c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?</p>				X
<p><u>d) Result in the loss of forest land or conversion of forest land to non-forest use?</u></p>				X
<p>e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?</p>				X

DISCUSSION: The site is not classified as agricultural or forest land and therefore is not contracted under the provisions of the Williamson Act. Development of the site as a truck wash is consistent with the truck oriented commercial services established in the surrounding area.

CONCLUSION: Development of the site as a truck wash will not impact agricultural or forest land.

<i>Issues:</i>	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
III. AIR QUALITY -- Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:				
a) Conflict with or obstruct implementation of the applicable air quality plan?			X	
b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?			X	
c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?			X	
d) Expose sensitive receptors to substantial pollutant concentrations?			X	
e) Create objectionable odors affecting a substantial number of people?			X	

DISCUSSION: The Project area is located in the Northern Sacramento Valley Air Basin (*NSVAB*) which is one of the air “sub-basins” within the Sacramento Valley Air Basin. The other sub-basin is the Greater Sacramento Air region. The *NSVAB* encompasses Shasta, Tehama, Glenn, Butte, Colusa, Sutter, and Yuba counties. The basin’s principal geographic features include a large valley bounded on the north and west by the Coastal Mountain Range and on the east by the southern portion of the Cascade Mountain Range and the northern portion of the Sierra Nevada. The basin is about 200 miles long in a north-south direction, and has a maximum width of about 150 miles, although the valley floor averages only about 50 miles in width. The mountain ranges reach heights in excess of 6,000 feet with peaks rising much higher. The general elevation of the Project site is about 275 feet above mean sea level.

The area climate is characterized by hot, dry summers and cool, wet winters. During the summer months from mid-April to mid-October, significant precipitation is unlikely and temperatures range from daily maximums exceeding 100° Fahrenheit (°F) to evening lows in high 50s and low 60s. During the winter, highs are typically in the 60s with lows in the 30s. Wind direction is primarily along the valley due to the channeling effect of the mountains to either side of the valley. During the summer months, surface air movement is from the south, particularly during the afternoon hours. During the winter months, wind direction is more variable.

The quantity of air pollutant emissions generated within the *NSVAB* is small compared to the more densely populated areas such as the Sacramento and the San Francisco Bay areas. Nevertheless, the following characteristics of the *NSVAB* make it susceptible for the build-up of air pollution.

- Pollution generated in the broader Sacramento area and San Francisco Bay area can be transported northward into the *NSVAB*.
- The mountain ranges to the west, north, and east of the *NSVAB* act as horizontal barriers which restrict the flow of pollution out of the basin.
- The valley portion of the *NSVAB* (those areas below 1,000 feet elevation) is often subjected to temperature inversions that typically occur during cool, calm nights that restrict vertical mixing and dilution of pollutants.
- The typical clear skies and warm temperatures in the summer months promote the formation of the photochemical pollutant ozone.

The federal and state governments have enacted laws mandating the identification of areas not meeting the ambient air quality standards and development of regional air quality plans to eventually attain the standards. National ambient air quality standards are determined by the US EPA. The standards include both primary and secondary ambient air quality standards. Primary standards are established with a safety margin. Secondary standards are more stringent than primary standards and are intended to protect public health and welfare. States have the ability to set standards that are more stringent than the federal standards. As such, California established more stringent ambient air quality standards.

Federal and State air quality standards have been established for six ambient air pollutants, commonly referred to as “criteria” air pollutants standards based on a comprehensive review of their health effects. The criteria air pollutants for which federal and state ambient standards have been established include ozone (O₃), carbon monoxide (CO), nitrogen monoxide (NO), sulfur dioxide (SO₂), suspended particulate matter (PM₁₀), fine particulate matter (PM_{2.5}) and lead (Pb). In this analysis, O₃ is evaluated by assessing emissions of O₃ precursors: reactive organic gases (ROG) and Nitrogen Oxides (NO_x).

Under CEQA, as well as the State and Federal Clean Air Acts, non-attainment of any air quality standard is considered to be an impact. Incremental emissions of non-attainment pollutants are generally considered to be cumulatively significant, because they contribute to non-attainment. Construction and grading activities will generate dust adding to PM₁₀ emissions. Heavy equipment and vehicular trips by construction personnel will contribute vehicular emissions as well. Construction cleanup often entails burning of trees, brush, and other wastes. These would be considered short term impacts.

The following mitigation measures will be implemented to reduce any impacts the project will have on air quality to a Less than Significant level.

Mitigation Measure III. B. 1

FUGITIVE DUST PERMIT

Prior to commencement of any type of construction activities the applicant must submit a construction emission dust/control plan and obtain a Fugitive Dust Control Permit from the Tehama County Air Pollution District and comply with the conditions of approval.

Mitigation Measure III. B. 2

OPEN BURNING

No opening burning shall occur on this parcel unless a special land clearing permit is obtained from the Tehama County Air Pollution Control District.

Mitigation Measure III. C. 1

SPRINKLE EXPOSED SOILS.

During construction, unprotected or bare soils, including inactive storage piles, shall be watered a minimum of 2 times per day to minimize wind erosion. Frequency should be based upon the type of operation, soil, and wind exposure.

Mitigation Measure III. C. 2

COVER EXPOSED SOILS. Areas denuded by construction activities and not scheduled for development for an indefinite period shall be seeded or covered by impervious materials to minimize water and wind erosion prior to the beginning of the rainy season (October 15th).

CONCLUSION: The identified mitigation measures reduce construction related impacts to a less than significant level and will also assure that impacts to sensitive receptors such as existing commercial business in the area are not significant. Since the project site is located in an area developed with truck services there will be no long term air quality impacts associated with relocating the existing truck wash. In addition to these mitigation measures staff will recommended that the following conditions of approval.

RECOMMENDED CONDITIONS OF APPROVAL:

GRADING PLANS. Complete grading plans shall be submitted for approval by the City Engineer.

STREET CLEANING. Paved City roadways leading to or from the project area shall be swept or washed at the end of each day as necessary to remove excessive accumulations of silt and/or mud, which may have accumulated as the result of construction activities.

<i>Issues:</i>	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
IV. BIOLOGICAL RESOURCES -- Would the project:				
a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?				X
b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or US Fish and Wildlife Service?				X
c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?				X
d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?				X
e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?				X
f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?				X

DISCUSSION: The site is located in an area developed with a variety of commercial services for truck and auto travel along Interstate 5. Portions of the site are covered with road base and gravel and previously used for truck and trailer parking. No streams, drainage patterns or other sensitive natural communities will be impacted by relocating the existing truck wash to this site.

CONCLUSION: Since the surrounding area is developed with commercial services and this project will be an "infill" of commercial development Biological Resources will not be impacted.

<i>Issues:</i>	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
V. CULTURAL RESOURCES -- Would the project:				
a) Cause a substantial adverse change in the significance of a historical resource as defined in 15064.5?		X		
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to 15064.5?		X		
c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?		X		
d) Disturb any human remains, including those interred outside of formal cemeteries?		X		

DISCUSSION: Historical recognized environmental conditions (HRECs) are defined by the ASTM Practice E1527-00 as an environmental condition which in the past would have been considered a recognized environmental condition, but which may or may not be considered a recognized environmental condition currently. The site is not located in an area where cultural resources have been located or known to be located. Portions of the site have recently been used as a truck turnaround and parking area. Should any type of cultural resources be unearthed, as a result of construction activities, they could be disturbed or damaged. Therefore, the following mitigation measures will be implemented to prevent significant impacts associated with development of the site.

Mitigation Measure V. A. 1

CULTURAL RESOURCES. If subsurface deposits believed to be cultural in origin are discovered during construction, then all work must halt within a 100-foot radius of the discovery, and the City of Corning notified. A qualified professional archaeologist, meeting the Secretary of the Interior's Professional Qualification Standards for prehistoric and historic archaeologist, shall be retained to evaluate the significance of the find. Work cannot continue at the discovery location until the archaeologist conducts sufficient research and data collection to make a determination that the resource is either 1) not cultural in origin; or 2) not potentially significant. If a potentially-eligible resource is encountered, then the archaeologist, lead agency, and project proponent shall arrange for either 1) total data recovery as a mitigation, or, preferably, 2) total avoidance of the resource, if possible. The determination shall be formally documented in writing and submitted to the lead agency as verification that the provisions in CEQA for managing unanticipated discoveries have been met.

Mitigation Measure V. D. 1

HUMAN REMAINS. If human remains, or remains that are potentially human, are discovered during project construction or implementation, all work must stop within a 100-foot radius of the find. The construction supervisor must notify the Corning Police Department immediately, and take appropriate action to ensure that the discovery is protected from further disturbance or vandalism.

Conclusion: Implementation of these mitigation measures will reduce any impacts to Cultural Resources to a Less than Significant level.

<i>Issues</i>	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
VI. GEOLOGY AND SOILS -- Would the project:				
a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:				
i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.			X	
ii) Strong seismic ground shaking?			X	
iii) Seismic-related ground failure, including liquefaction?			X	
iv) Landslides?				X
b) Result in substantial soil erosion or the loss of topsoil?		X		
c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?			X	
d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?			X	
e) Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?			X	

DISCUSSION: The site is not in the Alquist-Priolo Earthquake Fault Zone and there are no known active faults within the vicinity of the project. The Safety Element of the Corning General Plan indicates that there is very little threat of a potentially damaging earthquake within the City. The site and surrounding area is relatively flat and there is no risk of landslides or liquefaction. Therefore, there are no potential impacts, or risks to people or structures, associated with development of the site.

Development of the site will require grading and re-leveling for building pads, ingress and egress to the truck wash, parking lots, landscaped areas and drainage. Construction activities where clearing, grading, filling,

pavement and building construction and excavation result in a land disturbance of one or more acres require a Construction Storm Water Permit issued by the California Regional Water Quality Control Board (RWQCB). The permit requires that a Storm Water Pollution Prevention Plan (SWPPP) be prepared prior to construction activities. The SWPPP is used to identify potential pollutants (such as sediment and earthen materials, chemicals, construction materials, etc.) and to describe practices to eliminate or reduce those pollutants from entering surface waters. To assure that the project complies with the RWQCB requirements and prevent soil erosion and the loss of topsoil the following mitigation measures will be implemented.

Mitigation Measure VI. B. 1

STORMWATER PERMIT. Applicant shall apply for and obtain a “Construction Activities Storm Water General Permit” from the State Water Resources Control Board, Central Valley Regional Water Quality Control Board.

Mitigation Measure VI. B. 2

STORMWATER POLLUTION PREVENTION PLAN. Prior to any site disturbance or earthmoving activities on or adjacent to the site, a construction period and post-construction period Storm Water Pollution Prevention Plan (SWPPP) shall be prepared and presented to the Central Valley Regional Water Quality Control Board and approved by the City of Corning. The objective of the plan shall be no net loss of soil (above an undisturbed natural, stable background state) from the site due to erosion. All requirements of the post construction period SWPPP shall be completed as part of the required improvement plans and shall be maintained in the same manner.

CONCLUSION: The truck wash will be required to connect to city sewer and water service therefore the soil will not have to support individual septic systems. Potential impacts have been mitigated to a Less than Significant level.

RECOMMENDED CONDITION OF APPROVAL:

SOILS INVESTIGATION: Prior to the issuance of a building permit for the truck wash facility the applicant shall submit a soils investigation by a registered engineering geologist or civil engineer to determine if expansive soils requiring special foundation design is necessary. The developer shall provide: 1) certification assuring adequate compaction of filled lots in accordance with the Uniform Building Code; and 2) for those lots with expansive soils, certification that the engineered foundation plans comply with building code requirements.

<i>Issues:</i>	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
VII. GREENHOUSE GAS EMISSIONS -- Would the project:				
a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?		X		
b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?			X	

DISCUSSION: California is a substantial contributor of global greenhouse gases, emitting over 400 million tons of carbon dioxide (CO₂) each year. Climate studies indicate that California is likely to see an increase of three to four degrees Fahrenheit over the next century. Methane is also an important greenhouse gas that potentially contributes to global climate change. Greenhouse gases are global in their effect, which is to increase the earth's ability to absorb heat in the atmosphere. Because primary greenhouse gases have a long lifetime in the atmosphere, accumulate over time, and are generally well mixed, their impact on the atmosphere is mostly independent of the point of emission.

Assembly Bill 32 (Global Warming Solutions Act) (AB-32) was passed by the California Legislature on August 31, 2006. It requires the State's global warming emissions to be reduced to 1990 levels by 2020. The reduction would be accomplished through an enforceable statewide cap on global warming emissions that would be phased in starting in 2012. On or before June 30, 2007, the California Air Resources Control Board (CARB) is required to publish a list of discrete greenhouse gas emissions that can be reduced. Emission reductions shall include carbon sequestration projects and best management practices that are technologically feasible and cost-effective. However, AB 32 did not provide thresholds or methodologies for analyzing a project's impacts regarding global climate change and primarily provides a timeframe for establishing plans, policies, and studies to address global climate change.

Executive Order S-3-05 also recognized the importance of preparedness in that it directed the Secretary of the California Environmental Protection Agency (Cal EPA) to lead an effort to evaluate the impacts of climate change on California and to examine adaptation measures that would best prepare the state to respond to the adverse consequences of climate change. In response to S-3-05, the Climate Action Team (CAT) was convened, which comprised representatives from Cal EPA, CARB, Integrated Waste Management, California Energy Commission, and several other state departments. The CAT prepared the *Climate Action Team Report to Governor Schwarzenegger and the Legislature* (dated March 2006), which provides an overview of

scientific evidence regarding climate change as well as potential effects on California. The report also provides recommendations regarding strategies the state should pursue to reduce climate change emissions.

In light of such legislation such as AB 32 and Executive Order S-3-05, there has been much debate regarding the analysis of global climate change in CEQA documents. On April 13, 2009, the Governor's Office of Planning and Research (OPR), submitted to the Secretary for Natural Resources its proposed amendments to the state CEQA Guidelines for greenhouse gas emissions, as required by Senate Bill 97 (Chapter 185, 2007). These proposed CEQA Guideline amendments will provide guidance to public agencies regarding the analysis and mitigation of the effects of greenhouse gas emissions in draft CEQA documents. The Natural Resources Agency will conduct formal rulemaking in 2009, prior to certifying and adopting the amendments, as required by Senate Bill 97. OPR expects that guidelines will be adopted on or before January 1, 2010 consistent with Public Resources Code section 21083.05 which was added to CEQA by SB 97. Provided in the proposed CEQA Guideline amendments is a checklist for greenhouse gas emissions that is utilized in this Initial Study even though they are not formally adopted.

In addition to the CEQA Guideline amendments, air districts have traditionally provided guidance to local lead agencies on evaluating and addressing air pollution impacts from projects subject to CEQA. Recognizing the need for a common platform of information and tools to support decision makers as they establish policies and programs for greenhouse gasses and CEQA, the California Air Pollution Control Officers Association (CAPCOA) has prepared a white paper reviewing policy choices, analytical tools, and mitigation strategies. This paper is intended to serve as a resource for public agencies as they establish agency procedures for reviewing greenhouse gas emissions from projects under CEQA. In order to provide a threshold for CO₂ and CO₂ equivalents for purposes of CEQA analysis, the TCAPCD has established a threshold of 900 metric tons per year, in accordance with the CAPCOA document.¹ The 900 metric ton screening criteria (CO₂ or CO₂ equivalents generated annually) being used by the TCAPCD is a conservative criterion for determining if a project requires further analysis and mitigation with regard to climate change.

Since this project is the relocation of an existing established business it would be easy to assume that there would be no impacts to greenhouse gases. It is unknown if the existing building, currently occupied by Blue Beacon Truck Wash, will remain on the site where it is located and if it does what the potential use will be. The lease agreement with Blue Beacon and the property owner prevents this building from every being used as a truck wash, the change in use will require the approval of a Use Permit, and additional CEQA analysis prior to occupancy. To assure that the new truck wash facility mitigates impacts to greenhouse gases the following design and energy efficient mitigation measures will be implemented:

Mitigation Measure VII. A. 1

BUILDING EFFICIENCY. Increase building energy efficiency rating by 10% above what is required by Title 24 requirements. This can be accomplished in a number of ways (increasing attic, wall or floor insulation, etc.)

Mitigation Measure VII. A. 2

THERMAL EFFICIENCY. Improvement of thermal efficiency of the office area by reducing the thermal load with automated and timed temperature controls.

CONCLUSION: Implementing the mitigation measures will assure that the office space within the truck wash facility will be more energy efficient than the current site and possibly reduce the greenhouse gases produced by the current operation.

<i>Issues:</i>	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
VIII. HAZARDS AND HAZARDOUS MATERIALS Would the project:				
a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?			X	
b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?		X		
c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?				X
d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?				X
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?				X
f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?				X
g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?				X
h) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?				X

DISCUSSION: According to Section 25117 of the *California Health and Safety Code*, a hazardous material is any material that, because of its quantity, concentration, or physical or chemical characteristics, poses a significant present or potential hazard to human health or the environment if released into the workplace or the

environment. Hazardous substances can take the form of a solid, dust, liquid, or fume and exhibit any of the criteria set forth in 22 CCR, Chapter 30, Article 11. A list of wastes that are presumed hazardous is presented in Chapter 30, Article 9 of Title 22. Hazardous waste criteria include toxicity, ignitability, reactivity, and corrosivity.

The project site is located adjacent to I-5 where there is a possibility that vehicles transporting hazardous materials could experience an accident impacting the proposed facility. The structures housing the truck wash bays, equipment room and office space will be located approximately 300 feet east of the I-5 right-of-way. The 300 feet provides adequate separation should there be an accident where hazardous material is spilled or released due to an accident on I-5. Also the site is accessed by Hwy 99-W which would provide an emergency escape route should it be necessary.

The following mitigation measure will be implemented to assure that if any hazardous materials are stored on site such as fuels, cleaning solvents, etc., the storage of them is in compliance with applicable storage requirements of the Tehama County Environmental Health Department.

Mitigation Measure VIII. B. 1

HAZARDOUS MATERIALS STORAGE. The operators of the truck wash must submit a signed Hazardous Materials general information questionnaire to the Tehama County Department of Environmental Health and if business handles hazardous materials above the threshold quantities a business emergency response plan and inventory shall be submitted to the Department of Environmental Health at least 30 days prior to commencing operation.

The project site is outside of the Corning Airport Land Use Plan and there are no private airports located within the vicinity of the project.

Regarding wildland fire hazards, the site is surrounded by existing commercial development. The fuel necessary to feed a large wildland fire is not existent within or adjacent to the proposed site. The City has a weed abatement ordinance to reduce the accumulation of weeds and other flammable materials within the City.

CONCLUSION: The only potential hazard associated with locating the truck wash at this site has been mitigated to a Less than Significant Level.

<i>Issues:</i>	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
IX. HYDROLOGY AND WATER QUALITY -- Would the project:				
a) Violate any water quality standards or waste discharge requirements?			X	
b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?			X	
c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?			X	
d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?			X	
e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?			X	
f) Otherwise substantially degrade water quality?			X	
g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?				X
h) Place within a 100-year flood hazard area structures which would impede or redirect flood flows?			X	
i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?				X

j) Inundation by seiche, tsunami, or mudflow?				X
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DISCUSSION: The truck wash facility will be connected to the City of Corning waste water disposal system where the effluent will be piped to the waste water treatment plant and treated before being released into the Sacramento River. Effluent treated and released from the plant must comply with waste discharge requirements imposed by the Regional Water Control Board. The truck wash will also connect to the City of Corning Community Water System that has adequate capacity to serve the proposed use without having to expand its system. Stormwater detention basins will be constructed on the site to capture stormwater on site which will increase the groundwater recharge ability of the site.

The existing drainage patterns of the site will not be altered and no stream or rivers will be altered or flow increased due to the site drainage. Butler Engineering Group has submitted storm drainage calculations showing that the storm drainage detention ponds are adequate, and in fact more restrictive than the drainage requirements and standards imposed by the City of Corning. Butler calculated 10, 25, & 100 year storm events based on 4 hour storm duration. The on-site stormwater detention ponds have been calculated comparing the 10 year pre-construction storm event compared to the 100 year post-development storm event. This analysis created a more conservative approach and produces a larger on-site stormwater storage capacity.

Water draining from the truck wash bays will be piped through a oil separator prior to being released into the drainage system. The front portion of the parcel is within the 100 year floodplain but all buildings and parking areas will be constructed outside of the floodplain therefore there will be no exposure of structures or people to flooding nor will the structures impede flood flows.

Recommended Conditions of Approval:

FINISHED GRADE Finished grade must be graded to direct runoff to stormwater drain facilities within the public right-of way or established drainage facilities (detention basins) constructed on the parcel. No lot-to-lot runoff is permitted.

DETENTION BASINS The finished grading plan and detention basin size must be approved by the City Engineer prior to the commence of any grading of the site.

CONCLUSION: The City of Corning has sufficient capacity in its waste water disposal system and community water system to serve the proposed truck wash facility. The design of the drainage system is very conservative and the detention ponds will reduce the post-development stormwater runoff to less than the pre-developed runoff. Development of the site as a truck wash will not have a significant impact on hydrology or water quality.

<i>Issue</i>	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
X. LAND USE AND PLANNING - Would the project:				
a) Physically divide an established community?				X
b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?				X
c) Conflict with any applicable habitat conservation plan or natural community conservation plan?				X

DISCUSSION: The *Land Use Element* of the City's General Plan sets forth the City's policies for guiding local development. These policies, together with existing zoning, establish the amount and distribution of permitted land uses within each zone, and sets forth development standards with which the permitted land uses must comply.

The development of a truck wash facility does not conflict with the Highway 99W Corridor Specific Plan which was adopted as a mechanism for incorporating creative design into a mix of retail commercial, office, warehousing, and light manufacturing activities. Mitigation Measures I.C.1 landscaping the site to comply with the specific plan.

Relocating the Blue Beacon truck wash in the South Ave. interchange area that is currently developed with truck service facilities, will not physically divide an established community or conflict with any applicable habitat or natural community conservation plans.

CONCLUSION: Issuance of a use permit for a commercial business that is geared toward servicing trucks that travel along I-5 is consistent with the Hwy 99W Corridor Specific Plan and existing zoning designations therefore there are no impacts to Land Use and Planning.

<i>Issues:</i>	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
XI. MINERAL RESOURCES -- Would the project:				
a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?				X
b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?				X

DISCUSSION: According to the *City of Corning General Plan*, mineral extraction and construction accounted for four percent of the employment in Tehama County in 1983. Fourteen mineral resources have been identified in the County, including aragonite, borax, chalcopryrite, chromite, copper, cristobalite, galena, garnet, opal, pectolite, penninite, sassolite, and Wallsonite. The most plausible mineral for future development is chromite, used for steel production. In Tehama County, most of the chromite deposits are found in the western section of the County, and would therefore have little or no effect on the City.

Currently, there are no extraction activities taking place within the Project parcels or within the City. Review of California Geological Survey Surface Mining and Reclamation Act of 1975 (SMARA) mineral classification maps does not identify mineral resource deposits that could be impacted. There are *no impacts* to mineral resources.

CONCLUSION: The proposed Project will result in no impacts on mineral resources.

<i>Issues:</i>	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
XII. NOISE Would the project result in:				
a) Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?			X	
b) Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?			X	
c) A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?		X		
d) A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?		X		
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?				X
f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?				X

DISCUSSION: The Highway 99W Corridor Specific Plan contains a noise element adopted to help protect health and welfare of the area by promoting development which is compatible with accepted noise standards. The human ear is subject to a wide range of sound intensities and people hear changes in sound in proportion to those intensities. The decibel (db) scale is a logarithmic scale used to compress this range. The threshold of human hearing corresponds roughly to 0 db. The "A" weighting scale, that which most closely resembles human hearing, is used in the specific plan and is noted by the symbol dBA.

In the specific plan, the time varying character of environmental noise is described as Ldn. This is a statistical weighting of daytime and nighttime noises and is used as the basis of noise impact evaluation and for land use planning criteria.

Ambient noise levels constitute the composite from all sources far and near. In this context, the ambient noise level constitutes the normal or existing level of environmental noise at a given location. Parameters used when estimating traffic noise relate to the traffic, the roadway, and the receiver. Traffic parameters affecting noise are the number and type of vehicles passing a point during a particular time period and the average speed of the vehicles. Roadway variables include its surface, gradient, and geometry.

The General Plan Noise Element identified that normally acceptable range for office and commercial land uses is an Ldn value below 70 dbA (Ldn values of 67.5 to 77.5 are conditionally acceptable). Freeway noise increases as the number and average speed of automobiles on it increases. For example, if the automobile traffic volume doubles, the noise level from those autos increases by about 3 dBA. As the population of California and other western states increases the traffic along I-5 will increase which will result in increased noise levels at this location.

The noise contour map within the specific plan indicates that when the plan was adopted in 1997 present noise contours along I-5 were 60 dBA at 300 feet, 65 dBA at 170 feet, and 70 dBA at 80 feet. The Average Daily Trip (ADT) for I-5 was at 25,000 vehicles in the peak month in 1997. The ADT on I-5 has increased since 1997 but has not doubled which would increase these figures by 3dBA. Placement of a the truck wash facilities approximately 300 feet from I-5 would expose people to a noise level from I-5 of between 60 dBA to 70 dBA which are acceptable ranges as established in the General Plan and Specific Plan.

North of the site is an established wholesale and retail irrigation supply company and south of the site is a small motel, restaurant, construction activities associated with the development of the site will temporarily increase the ambient noise levels above the existing levels. The following mitigation measure will be implemented to reduce the impact of these short-term construction related noises.

Mitigation Measure XI. D. 1

CONSTRUCTION HOURS. Excavation and construction work shall occur only between the hours of 7:00 AM to 7:00 PM, Monday through Friday, and between the hours of 8:00 AM to 6:00 PM on weekends and federally observed holidays.

Mitigation Measure XI. D. 2

CONSTRUCTION EQUIPMENT. The primary contractor shall be responsible for ensuring that all construction equipment is properly tuned and maintained. When feasible, existing power sources, such as power poles, or clean fuel generators should be used, rather than temporary power generators.

Minimize idling time to 10 minutes.

The following mitigation measure will be implemented to reduce the permanent increase in ambient noise levels in the project vicinity, above levels existing without the project, to a Less than Significant Level.

Mitigation Measure XI. C. 1

NOISE REDUCTION.

A minimum two feet high berm along the north and south property lines, with trees planted on top as shown on the site plans submitted with the Use Permit application, must be installed prior to the City issuing a Certificate of Occupancy for the truck wash.

The Airport Noise Contour Map shows that the site is well outside the 55 CNEL noise contour, which is well below the acceptable range for commercial land uses, therefore, noise from the airport will not significantly impact the project site.

CONCLUSION: Short-term construction noise and noise associated with relocating the truck wash to this location have been mitigated to a Less than Significant level. Noise impacts from I-5 are within the levels acceptable for commercial development in the General Plan.

<i>Issues:</i>	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
XIII. POPULATION AND HOUSING -- Would the project:				
a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?				X
b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?				X
c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?				X

DISCUSSION: The project is the relocation of an existing truck wash facility in an area designated for commercial and light industrial uses geared at providing services for auto and truck travel along Interstate 5. The project will not induce substantial population growth or displace existing housing in the area.

CONCLUSION: The project is commercial development and will not have an impact to the population or housing needs within the City of Corning.

<i>Issues:</i>	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
XIV. PUBLIC SERVICES				
a) Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:				
Fire protection?			X	
Police protection?			X	
Schools?			X	
Parks?			X	
Other public facilities?			X	

DISCUSSION: Commercial development impacts numerous public services. To mitigate these impacts the Coming School Districts and the City of Coming have adopted and implemented Development Impact fees. These fees, payable when building permits are issued, were implemented to mitigate the impacts created by new commercial and residential development. This project will be subject to the schools and cities established fees which will mitigate the impacts to these services.

RECOMMENDED CONDITION OF APPROVAL:

DEVELOPMENT IMPACT FEES. Development of the truck wash at this location is subject to imposed in order to lessen new development's impacts on City facilities and services. These fees shall be assessed and payable prior to issuance of a Certificate of Occupancy. Costs incurred for improvements to "backbone" infrastructure facilities shall be credited against development.

CONCLUSION: Existing impact fees mitigate the impacts on Public Services to a Less than Significant level.

<i>Issues:</i>	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
XV. RECREATION --				
a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?				X
b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?				X

DISCUSSION: The proposed truck wash will replace an existing truck wash, currently operated by Blue Beacon International, Inc., located along the south side of South Ave. The lease agreement with the property owner prevents the existing site from being reopened as a truck wash once Blue Beacon vacates the facility.

CONCLUSION: Relocating the truck wash facility within the South Ave. interchange area will not have an impact on the recreational facilities in or around the City of Coming.

<i>Issues:</i>	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
XVI. TRANSPORTATION/TRAFFIC -- Would the project:				
a) Cause an increase in traffic which is substantial in relation to the existing traffic load and capacity of the street system (i.e., result in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads, or congestion at intersections)? Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?			X	
b) Exceed, either individually or cumulatively, a conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?			X	
c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?				X
d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?			X	
e) Result in inadequate emergency access?				X
f) Result in inadequate parking capacity?			X	
g) Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities supporting alternative transportation (e.g., bus turnouts, bicycle racks)?				X

DISCUSSION: The project site is located where it will primarily be served by the South Avenue interchange and South Avenue / Hwy 99W intersection. The Interstate 5 off-ramps and the South Avenue / Hwy 99W

intersection were recently improved by adding additional width and new travel lanes and installing additional signalization for movement throughout the area. This project was Phase one of two planned phases of improvements to the South Avenue interchange and overpass. These improvements are being made to accommodate the existing and future truck and vehicle traffic in this area.

Since this project is the relocation of an existing truck wash facility and the road infrastructure in this area has been improved to accommodate truck traffic the project will not cause an increase in traffic which is substantial in relation to the existing traffic load and capacity of the street system or result in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads, or congestion at intersections.

The following recommended conditions of approval for the Use Permit will require street improvements to Hwy 99W to assure that access to the truck wash facility is in compliance with City standards.

Recommend conditions of approval:

HIGHWAY 99W IMPROVEMENTS: Construct the adjacent (west) half width of Highway 99W in accordance with the adopted standard (including acceleration lane, travel lane, median turn lane) and complete asphalt overlay of a 14 foot travel lane on the opposite (east) half width along property frontage with appropriate tapering north and south of the project site.

CURB, GUTTER, & SIDEWALK IMPROVEMENTS: Install curb, gutter and sidewalk, with approved handicap ramps in accordance with adopted Highway 99W Specific Plan standard.

THERMOPLASTIC STRIPING Apply thermoplastic striping for median turn lane and acceleration lane as directed by the City Engineer.

PUBLIC IMPROVEMENT STANDARDS All public improvements shall be constructed in accordance with the Subdivision Ordinance of the City of Corning and required Public Works Standards.

The project will not result in a change in air traffic patterns and the frontage along Hwy 99W and access points for truck traffic provide adequate emergency access. The site plan submitted with the Use Permit application provides adequate parking spaces for employees and customers of the truck wash.

CONCLUSION: Relocating the existing truck wash facility from its current location to this proposed site will not have a significant impact to the existing transportation facilities or substantially increase the traffic in the South Avenue / Hwy 99W intersection area.

<i>Issues:</i>	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
XVII. UTILITIES AND SERVICE SYSTEMS Would the project:				
a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?			X	
b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?			X	
c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?			X	
d) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?			X	
e) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project=s projected demand in addition to the provider=s existing commitments?			X	
f) Be served by a landfill with sufficient permitted capacity to accommodate the project=s solid waste disposal needs?			X	
g) Comply with federal, state, and local statutes and regulations related to solid waste?			X	

DISCUSSION: The truck wash will be connected to the city water and sewer systems currently serving the site and surrounding commercial development. Both systems have sufficient capacity to serve the truck wash and no new systems will have to be constructed or expanded. As previously discussed the on-site stormwater detention ponds produce a larger on-site stormwater storage capacity than currently exists therefore development of the truck wash will not require the construction of new storm water drainage facilities or expansion of existing facilities

The Tehama County Landfill has sufficient capacity to accommodate the project.

The following conditions of approval will be recommended to assure that development of the site complies with city codes and does not negatively impact the systems.

Recommended Conditions Of Approval:

WATER & SEWER CONNECTIONS Water and sewer connections shall be completed in accordance with Public Works Specifications. Install manhole at sewer lateral connection in Highway 99W.

WATER METERS All water meters to be Sensus compound meters to register in gallons.

WATER CONNECTIONS Water service connected to water main in Highway 99W, as per Public Works Specifications as shown on the Improvement Plans submitted with the application and dated 10/04/10

SEWER CONNECTIONS Sewer service connection to join gravity sewer truck line within Highway 99W right-of-way in accordance with Public Works Specifications as shown on Improvement Plans submitted with application and dated 10/04/10.

ELECTRICAL SERVICE Electrical service to the site shall be undergrounded from the existing pole on east side of Highway 99W as shown on the Improvement Plans, Utility Plan sheet UP5, submitted with the application and dated 10/04/10.

STREET LIGHTING Two street lights, positioned near the northeast and southeast property corners, respectively, shall be installed in accordance with Public Works Standards SL-1 and the City's Land Division Standards. Final location shall be shown on the plans for public improvements, and approved by the City Engineer.

CONCLUSION: Existing utilities and service systems will not be significantly impacted by the project.

<i>Issues:</i>	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
XVIII. MANDATORY FINDINGS OF SIGNIFICANCE --				
a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?			X	
b) Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?			X	
c) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?			X	

DISCUSSION: The project is located adjacent to Interstate 5 in an area currently developed with a variety of commercial services for truck and automobile fueling and service. Relocation of an existing truckwash facility to the proposed site will not degrade the quality of the environment, have cumulatively considerable impacts or have any adverse impacts on people in or around the facility.

CONCLUSION: Any potentially significant impacts associated with relocating the truck wash facility to this location have been identified and mitigated to a Less than Significant Level.

REFERENCE DOCUMENTS

The following studies, letters or memorandums were utilized to identify potential impacts, conditions and mitigation measures:

City of Corning General Plan

City of Corning Zoning Code

Highway 99w Corridor Specific Plan

Contact with Tim Potanovic, Director Tehama County Environmental Health Department

Information submitted by Butler Engineering Group

CEQA guidelines from the Tehama County Air Pollution Control District

LIST OF PREPARERS:

John Stoufer

Planning Director

City of Corning

MITIGATION MONITORING PROGRAM

Use Permit 2010-160

Blue Beacon International, Inc.
500 Graves Blvd
Salina, KS 67401
(705) 825-2221

The size and complexity of the proposed project require development of a formal mitigation monitoring program to ensure that monitoring is carried out in all stages. Monitoring is divided into three categories related to the timing of activities and implementation of mitigations.

1. Pre-Construction Mitigations (PC). These are activities that precede any actual land disturbance. Included among these mitigations are the development of drainage, erosion control and tree management plans. Also included are the delineation of any wetlands that may be subject to development impact and the establishment of Environmentally Sensitive Areas (ESAs) or Zones (ESZs) around archaeological sites and specimen oak trees.
2. Construction-Related Mitigations (DC). These include implementation of the drainage and erosion control plans, building setbacks from sensitive areas, and all other measures required to reduce the impacts of construction and development.
3. Ongoing Mitigations (OG). These include the maintenance programs necessary to ensure long-term control of erosion, protection of surface water quality in runoff, and protection of the wildlife and wildlife habitat resources on the project.

Monitoring will be the responsibility of various county and state agencies, although the physical inspections may be delegated to a private company or individuals chosen by these agencies and/or an environmental coordinator. All costs of mitigation monitoring will be borne by the developers, who are usually required to deposit money with the county or state agency in advance of the required monitoring effort.

The following environmental mitigation measures were incorporated in the conditions of approval for this project in order to mitigate identified environmental impacts to a level of insignificance. For tentative maps, some mitigation measures must be completed prior to map recordation (PR). Others are implemented during permitting stages following map recordation (AR), or are ongoing mitigation measures. A completed and signed checklist for each mitigation measure indicates that the mitigation measure has been complied with and implemented, and fulfills the monitoring requirements with respect to Assembly Bill 3180 (PRC Section 21081.6).

Currently, the applicant is seeking approval of Use Permit 2010-160. A description of the pending project can be found in the initial study. Questions about this monitoring program should be directed to the City of Corning Planning Department.

ACRONYMS USED

CDFG	California Department of Fish and Game
CalTrans	California Department of Transportation
CDF	California Department of Forestry
CSD	Community Services District
CVRWQCB	Central Valley Regional Water Quality Control Board
DEV	Developer
HOA	Homeowners' Association
TCAPCD	Tehama County Air Pollution Control District
CBD	City of Corning Building Department
CFD	City of Corning Fire Department
CPLD	City of Corning Planning Department
CPD	City of Corning Police Department
CPWD	City of Corning Public Works Department
USACOE	United States Army Corps of Engineers
TCEH	Tehama County Environmental Health Department

Monitoring Phases

PC Pre-Construction

DC During Construction

OG Ongoing

BP During Building Permit Approval

Subdivision Map Phase (Tentative Maps)

PR Prior to Map Recordation

AR After Map Recordation

MITIGATION MONITORING PROGRAM

ISSUE: Aesthetics

IMPACT(S): The specific plan provides sign design guidelines in an attempt to safeguard life, health, property and public welfare, and to preserve the character of the City by regulating the size, height, design, quality of materials, construction, location, lighting and maintenance of all signs not enclosed within a building. To assure that development of the site as a truck wash complies with the landscaping and sign requirements of the specific plan and at the same time reduces the visual impacts to a Less than Significant level the following mitigation measure will be implemented:

Mitigation Measure I. C. 1

LANDSCAPING PLANS. Prior to commencing construction activities associated with the development of the site as a truck wash, the applicant or his engineer shall submit landscaping and signage plans that comply with the landscaping design guidelines and sign design guidelines of the Highway 99W Corridor Specific Plan. The landscaping plans must include irrigation plans that comply with the "Water Efficient Landscape Regulations" pursuant to Section 15.08.055 of the Corning Municipal Code. Both plans must be approved by city staff prior to installation and the sign installation and landscaping must be completed prior to the city issuing a certificate of occupancy for the truck wash buildings.

Implementing Agency: Project applicant

Monitoring Agency: CPD

Funding Source: Developer/Applicant

Phase of Monitoring: PC - OG

Performance Standards (standard for success): As determined by Monitoring Agencies.

Additional Notes: _____

COMPLIANCE VERIFIED

(see attached verification report)

DATE _____

MITIGATION MONITORING PROGRAM

ISSUE: Air Quality

IMPACT(S): Under CEQA, as well as the State and Federal Clean Air Acts, non-attainment of any air quality standard is considered to be an impact. Incremental emissions of non-attainment pollutants are generally considered to be cumulatively significant, because they contribute to non-attainment. Construction and grading activities will generate dust adding to PM10 emissions. Heavy equipment and vehicular trips by construction personnel will contribute vehicular emissions as well. Construction cleanup often entails burning of trees, brush, and other wastes. These would be considered short term impacts.

Mitigation Measure III. B. 1
FUGITIVE DUST PERMIT

Prior to commencement of any type of construction activities the applicant must submit a construction emission dust/control plan and obtain a Fugitive Dust Control Permit from the Tehama County Air Pollution District and comply with the conditions of approval.

Implementing Agency: Project applicant

Monitoring Agency: TCAPCD

Funding Source: Developer/Applicant

Phase of Monitoring: PC - DC - OG

Performance Standards (standard for success): As determined by Monitoring Agencies.

Additional Notes: _____

COMPLIANCE VERIFIED

(see attached verification report)

DATE _____

MITIGATION MONITORING PROGRAM

ISSUE: Air Quality

IMPACT(S): Under CEQA, as well as the State and Federal Clean Air Acts, non-attainment of any air quality standard is considered to be an impact. Incremental emissions of non-attainment pollutants are generally considered to be cumulatively significant, because they contribute to non-attainment. Construction and grading activities will generate dust adding to PM10 emissions. Heavy equipment and vehicular trips by construction personnel will contribute vehicular emissions as well. Construction cleanup often entails burning of trees, brush, and other wastes. These would be considered short term impacts.

Mitigation Measure III. B. 2

OPEN BURNING

No opening burning shall occur on this parcel unless a special land clearing permit is obtained from the Tehama County Air Pollution Control District.

Implementing Agency: Project applicant

Monitoring Agency: TCAPCD

Funding Source: Developer/Applicant

Phase of Monitoring: PC – DC - OG

Performance Standards (standard for success): As determined by Monitoring Agencies.

Additional Notes: _____

COMPLIANCE VERIFIED

(see attached verification report)

DATE _____

MITIGATION MONITORING PROGRAM

ISSUE: Air Quality

IMPACT(S): Under CEQA, as well as the State and Federal Clean Air Acts, non-attainment of any air quality standard is considered to be an impact. Incremental emissions of non-attainment pollutants are generally considered to be cumulatively significant, because they contribute to non-attainment. Construction and grading activities will generate dust adding to PM10 emissions. Heavy equipment and vehicular trips by construction personnel will contribute vehicular emissions as well. Construction cleanup often entails burning of trees, brush, and other wastes. These would be considered short term impacts.

Mitigation Measure III. C. 1

SPRINKLE EXPOSED SOILS.

During construction, unprotected or bare soils, including inactive storage piles, shall be watered a minimum of 2 times per day to minimize wind erosion. Frequency should be based upon the type of operation, soil, and wind exposure.

Implementing Agency: Project applicant

Monitoring Agency: TCAPCD

Funding Source: Developer/Applicant

Phase of Monitoring: PC - DC - OG

Performance Standards (standard for success): As determined by Monitoring Agencies.

Additional Notes: _____

COMPLIANCE VERIFIED

(see attached verification report)

DATE _____

MITIGATION MONITORING PROGRAM

ISSUE: Air Quality

IMPACT(S): Under CEQA, as well as the State and Federal Clean Air Acts, non-attainment of any air quality standard is considered to be an impact. Incremental emissions of non-attainment pollutants are generally considered to be cumulatively significant, because they contribute to non-attainment. Construction and grading activities will generate dust adding to PM10 emissions. Heavy equipment and vehicular trips by construction personnel will contribute vehicular emissions as well. Construction cleanup often entails burning of trees, brush, and other wastes. These would be considered short term impacts.

Mitigation Measure III. C. 2

COVER EXPOSED SOILS. Areas denuded by construction activities and not scheduled for development for an indefinite period shall be seeded or covered by impervious materials to minimize water and wind erosion prior to the beginning of the rainy season (October 15th).

Implementing Agency: Project applicant

Monitoring Agency: TCAPCD

Funding Source: Developer/Applicant

Phase of Monitoring: PC – DC - OG

Performance Standards (standard for success): As determined by Monitoring Agencies.

Additional Notes: _____

COMPLIANCE VERIFIED

(see attached verification report)

DATE _____

MITIGATION MONITORING PROGRAM

ISSUE: Cultural Resources

IMPACT(S): Historical recognized environmental conditions (HRECs) are defined by the ASTM Practice E1527-00 as an environmental condition which in the past would have been considered a recognized environmental condition, but which may or may not be considered a recognized environmental condition currently. The site is not located in an area where cultural resources have been located or known to be located. Portions of the site have recently been used as a truck turnaround and parking area. Should any type of cultural resources be unearthed, as a result of construction activities, they could be disturbed or damaged. Therefore, the following mitigation measures will be implemented to prevent significant impacts associated with development of the site.

Mitigation Measure V. A. 1

CULTURAL RESOURCES. If subsurface deposits believed to be cultural in origin are discovered during construction, then all work must halt within a 100-foot radius of the discovery, and the City of Corning notified. A qualified professional archaeologist, meeting the Secretary of the Interior's Professional Qualification Standards for prehistoric and historic archaeologist, shall be retained to evaluate the significance of the find. Work cannot continue at the discovery location until the archaeologist conducts sufficient research and data collection to make a determination that the resource is either 1) not cultural in origin; or 2) not potentially significant. If a potentially-eligible resource is encountered, then the archaeologist, lead agency, and project proponent shall arrange for either 1) total data recovery as a mitigation, or, preferably, 2) total avoidance of the resource, if possible. The determination shall be formally documented in writing and submitted to the lead agency as verification that the provisions in CEQA for managing unanticipated discoveries have been met.

Implementing Agency: Project applicant

Monitoring Agency: CPD

Funding Source: Developer/Applicant

Phase of Monitoring: PC - OG

Performance Standards (standard for success): As determined by Monitoring Agencies.

Additional Notes: _____

COMPLIANCE VERIFIED

(see attached verification report)

DATE _____

MITIGATION MONITORING PROGRAM

ISSUE: Cultural Resources

IMPACT(S): Historical recognized environmental conditions (HRECs) are defined by the ASTM Practice E1527-00 as an environmental condition which in the past would have been considered a recognized environmental condition, but which may or may not be considered a recognized environmental condition currently. The site is not located in an area where cultural resources have been located or known to be located. Portions of the site have recently been used as a truck turnaround and parking area. Should any type of cultural resources be unearthed, as a result of construction activities, they could be disturbed or damaged. Therefore, the following mitigation measures will be implemented to prevent significant impacts associated with development of the site.

Mitigation Measure V. D. 1

HUMAN REMAINS. If human remains, or remains that are potentially human, are discovered during project construction or implementation, all work must stop within a 100-foot radius of the find. The construction supervisor must notify the Corning Police Department immediately, and take appropriate action to ensure that the discovery is protected from further disturbance or vandalism.

Implementing Agency: Project applicant

Monitoring Agency: CPD

Funding Source: Developer/Applicant

Phase of Monitoring: PC - OG

Performance Standards (standard for success): As determined by Monitoring Agencies.

Additional Notes: _____

COMPLIANCE VERIFIED

(see attached verification report)

DATE _____

MITIGATION MONITORING PROGRAM

ISSUE: Geology & Soils

IMPACT(S): Development of the site will require grading and re-leveling for building pads, ingress and egress to the truck wash, parking lots, landscaped areas and drainage. Construction activities where clearing, grading, filling, pavement and building construction and excavation result in a land disturbance of one or more acres require a Construction Storm Water Permit issued by the California Regional Water Quality Control Board (RWQCB). The permit requires that a Storm Water Pollution Prevention Plan (SWPPP) be prepared prior to construction activities. The SWPPP is used to identify potential pollutants (such as sediment and earthen materials, chemicals, construction materials, etc.) and to describe practices to eliminate or reduce those pollutants from entering surface waters. To assure that the project complies with the RWQCB requirements and prevent soil erosion and the loss of topsoil the following mitigation measures will be implemented.

Mitigation Measure VI. B. 1

STORMWATER PERMIT. Applicant shall apply for and obtain a “Construction Activities Storm Water General Permit” from the State Water Resources Control Board, Central Valley Regional Water Quality Control Board.

Implementing Agency: Project applicant

Monitoring Agency: CVRWQCB

Funding Source: Developer/Applicant

Phase of Monitoring: _____

Performance Standards (standard for success): As determined by Monitoring Agencies.

Additional Notes: _____

COMPLIANCE VERIFIED

(see attached verification report)

DATE _____

MITIGATION MONITORING PROGRAM

ISSUE: Geology & Soils

IMPACT(S): Development of the site will require grading and re-leveling for building pads, ingress and egress to the truck wash, parking lots, landscaped areas and drainage. Construction activities where clearing, grading, filling, pavement and building construction and excavation result in a land disturbance of one or more acres require a Construction Storm Water Permit issued by the California Regional Water Quality Control Board (RWQCB). The permit requires that a Storm Water Pollution Prevention Plan (SWPPP) be prepared prior to construction activities. The SWPPP is used to identify potential pollutants (such as sediment and earthen materials, chemicals, construction materials, etc.) and to describe practices to eliminate or reduce those pollutants from entering surface waters. To assure that the project complies with the RWQCB requirements and prevent soil erosion and the loss of topsoil the following mitigation measures will be implemented.

Mitigation Measure VI. B. 2

STORMWATER POLLUTION PREVENTION PLAN. Prior to any site disturbance or earthmoving activities on or adjacent to the site, a construction period and post-construction period Storm Water Pollution Prevention Plan (SWPPP) shall be prepared and presented to the Central Valley Regional Water Quality Control Board and approved by the City of Corning. The objective of the plan shall be no net loss of soil (above an undisturbed natural, stable background state) from the site due to erosion. All requirements of the post construction period SWPPP shall be completed as part of the required improvement plans and shall be maintained in the same manner.

Implementing Agency: Project applicant

Monitoring Agency: CVRWQCB

Funding Source: Developer/Applicant

Phase of Monitoring: PC - OG

Performance Standards (standard for success): As determined by Monitoring Agencies.

Additional Notes: _____

COMPLIANCE VERIFIED
(see attached verification report)

DATE _____

MITIGATION MONITORING PROGRAM

ISSUE: Greenhouse Gases

IMPACT(S): Since this project is the relocation of an existing established business it would be easy to assume that there would be no impacts to greenhouse gases. It is unknown if the existing building, currently occupied by Blue Beacon Truck Wash, will remain on the site where it is located and if it does what the potential use will be. The lease agreement with Blue Beacon and the property owner prevents this building from every being used as a truck wash, the change in use will require the approval of a Use Permit, and additional CEQA analysis prior to occupancy. To assure that the new truck wash facility mitigates impacts to greenhouse gases the following design and energy efficient mitigation measures will be implemented:

Mitigation Measure VII. A. 1

BUILDING EFFICIENCY. Increase building energy efficiency rating by 10% above what is required by Title 24 requirements. This can be accomplished in a number of ways (increasing attic, wall or floor insulation, etc.)

Implementing Agency: Project applicant

Monitoring Agency: CBD

Funding Source: Developer/Applicant

Phase of Monitoring: PC - OG

Performance Standards (standard for success): As determined by Monitoring Agencies.

Additional Notes: _____

COMPLIANCE VERIFIED

(see attached verification report)

DATE _____

MITIGATION MONITORING PROGRAM

ISSUE: Greenhouse Gases

IMPACT(S): Since this project is the relocation of an existing established business it would be easy to assume that there would be no impacts to greenhouse gases. It is unknown if the existing building, currently occupied by Blue Beacon Truck Wash, will remain on the site where it is located and if it does what the potential use will be. The lease agreement with Blue Beacon and the property owner prevents this building from every being used as a truck wash, the change in use will require the approval of a Use Permit, and additional CEQA analysis prior to occupancy. To assure that the new truck wash facility mitigates impacts to greenhouse gases the following design and energy efficient mitigation measures will be implemented:

Mitigation Measure VII. A. 2

THERMAL EFFICIENCY. Improvement of thermal efficiency of the office area by reducing the thermal load with automated and timed temperature controls.

Implementing Agency: Project applicant

Monitoring Agency: CBD

Funding Source: Developer/Applicant

Phase of Monitoring: PC - OG

Performance Standards (standard for success): As determined by Monitoring Agencies.

Additional Notes: _____

COMPLIANCE VERIFIED

(see attached verification report)

DATE _____

MITIGATION MONITORING PROGRAM

ISSUE: Hazards & Hazardous Material

IMPACT(S): The following mitigation measure will be implemented to assure that if any hazardous materials are stored on site such as fuels, cleaning solvents, etc., the storage of them is in compliance with applicable storage requirements of the Tehama County Environmental Health Department.

Mitigation Measure VIII. B. 1

HAZARDOUS MATERIALS STORAGE. The operators of the truck wash must submit a signed Hazardous Materials general information questionnaire to the Tehama County Department of Environmental Health and if business handles hazardous materials above the threshold quantities a business emergency response plan and inventory shall be submitted to the Department of Environmental Health at least 30 days prior to commencing operation.

Implementing Agency: Project applicant

Monitoring Agency: TCEH

Funding Source: Developer/Applicant

Phase of Monitoring: PC - OG

Performance Standards (standard for success): As determined by Monitoring Agencies.

Additional Notes: _____

COMPLIANCE VERIFIED

(see attached verification report)

DATE _____

MITIGATION MONITORING PROGRAM

ISSUE: Noise

IMPACT(S): North of the site is an established wholesale and retail irrigation supply company and south of the site is a small motel, restaurant, construction activities associated with the development of the site will temporally increase the ambient noise levels above the existing levels. The following mitigation measure will be implemented to reduce the impact of these short-term construction related noises.

Mitigation Measure XI. D. 1

CONSTRUCTION HOURS. Excavation and construction work shall occur only between the hours of 7:00 AM to 7:00 PM, Monday through Friday, and between the hours of 8:00 AM to 6:00 PM on weekends and federally observed holidays.

Mitigation Measure XI. D. 2

CONSTRUCTION EQUIPMENT. The primary contractor shall be responsible for ensuring that all construction equipment is properly tuned and maintained. When feasible, existing power sources, such as power poles, or clean fuel generators should be used, rather than temporary power generators. Minimize idling time to 10 minutes.

Implementing Agency: Project applicant

Monitoring Agency: CPD & CPWD

Funding Source: Developer/Applicant

Phase of Monitoring: DC

Performance Standards (standard for success): As determined by Monitoring Agencies.

Additional Notes: _____

COMPLIANCE VERIFIED

(see attached verification report)

DATE _____

MITIGATION MONITORING PROGRAM

ISSUE: Noise

IMPACT(S): The noise contour map within the specific plan indicates that when the plan was adopted in 1997 present noise contours along I-5 were 60 dBA at 300 feet, 65 dBA at 170 feet, and 70 dBA at 80 feet. The Average Daily Trip (ADT) for I-5 was at 25,000 vehicles in the peak month in 1997. The ADT on I-5 has increased since 1997 but has not doubled which would increase these figures by 3dBA. Placement of a the truck wash facilities approximately 300 feet from I-5 would expose people to a noise level from I-5 of between 60 dBA to 70 dBA which are acceptable ranges as established in the General Plan and Specific Plan.

Mitigation Measure XI. C. 1

NOISE REDUCTION.

A minimum two feet high berm along the north and south property lines, with trees planted on top as shown on the site plans submitted with the Use Permit application, must be installed prior to the City issuing a Certificate of Occupancy for the truck wash.

Implementing Agency: Project applicant

Monitoring Agency: CPD

Funding Source: Developer/Applicant

Phase of Monitoring: DC - OG

Performance Standards (standard for success): As determined by Monitoring Agencies.

Additional Notes: _____

COMPLIANCE VERIFIED

(see attached verification report)

DATE _____